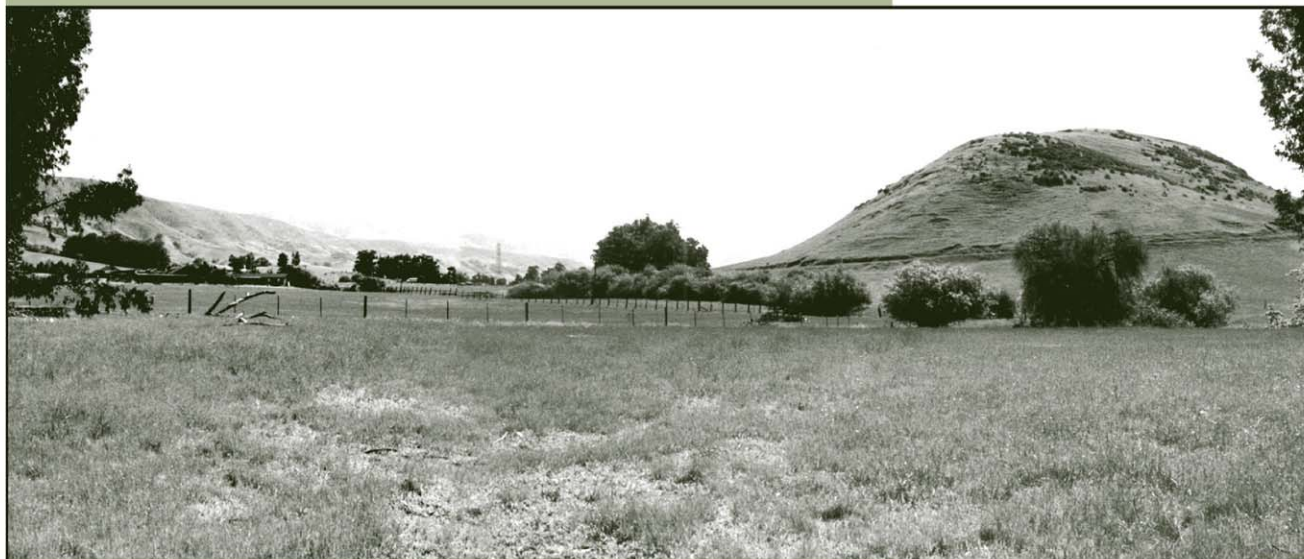
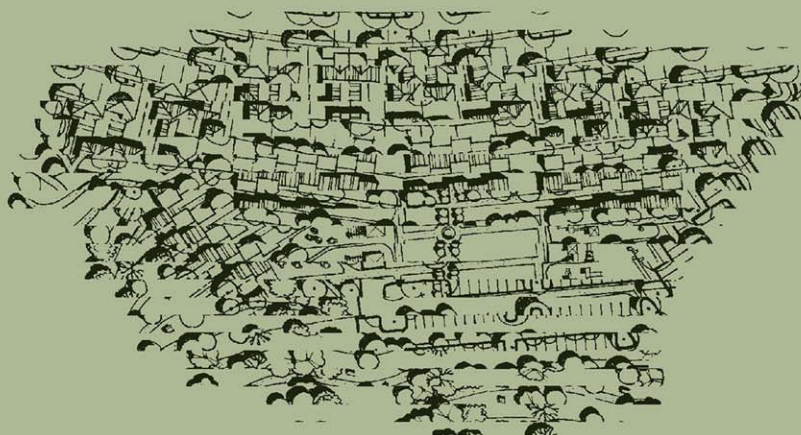


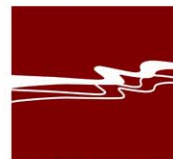
ORCUTT AREA SPECIFIC PLAN



MARCH 2010



PREPARED BY:



WALLACE GROUP®

CITY OF SAN LUIS OBISPO

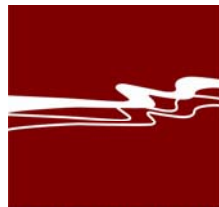
ORCUTT AREA SPECIFIC PLAN

MARCH 2010

PREPARED FOR:

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CONTENTS

- 1 INTRODUCTION 1-1**
 - 1.1 SCOPE OF SPECIFIC PLAN FOR ORCUTT PLAN AREA..... 1-1**
 - 1.2 PLAN GOALS AND CONSISTENCY WITH THE GENERAL PLAN..... 1-1**
 - 1.3 SPECIFIC PLAN FORMAT 1-2**
 - 1.4 LOCATION OF ORCUTT PLAN AREA 1-2**
 - 1.5 PLANNING AREA CHARACTER 1-2**
 - 1.6 SPECIFIC PLAN FEATURES 1-4**

- 2 CONSERVATION, OPEN SPACE, AND RECREATION 2-1**
 - 2.1 INTENT 2-1**
 - 2.2 BIOLOGICAL RESOURCES 2-1**
 - 2.3 RECREATION RESOURCES..... 2-9**
 - 2.4 SCENIC RESOURCES 2-15**
 - 2.5 ARCHAEOLOGICAL AND HISTORIC RESOURCES 2-17**

- 3 LAND USE AND DEVELOPMENT STANDARDS 3-1**
 - 3.1 DENSITY 3-1**
 - 3.2 LAND USE DESIGNATIONS AND ZONING 3-2**
 - 3.3 AFFORDABLE HOUSING 3-8**
 - 3.4 PUBLIC SAFETY 3-10**
 - 3.5 AIRPORT COMPATIBILITY..... 3-11**

- 4 COMMUNITY DESIGN**
 - 4.1 DESIGN QUALITY AND CHARACTER 4-1**
 - 4.2 COMMUNITY COMMERCIAL DESIGN 4-6**
 - 4.3 LANDSCAPING..... 4-13**
 - 4.4 LIGHTING..... 4-16**
 - 4.5 NOISE 4-17**
 - 4.6 PUBLIC ART 4-19**
 - 4.7 ENERGY CONSERVATION 4-19**

- 5 CIRCULATION 5-1**
 - 5.1 ARTERIALS 5-1**
 - 5.2 COLLECTOR STREETS 5-8**
 - 5.3 LOCAL STREETS 5-13**

5.4	PUBLIC TRANSPORTATION.....	5-13
5.5	PEDESTRIAN AND BICYCLE PATHS	5-15
5.6	CIRCULATION STANDARDS	5-16
6	PUBLIC UTILITIES.....	6-1
6.1	WATER SUPPLY	6-1
6.2	WASTEWATER FACILITIES.....	6-2
6.3	STORMWATER FACILITIES	6-2
6.4	ENERGY	6-14
6.5	TELECOMMUNICATIONS.....	6-14
7	PUBLIC SERVICES	7-1
7.1	EDUCATION.....	7-1
7.2	FIRE PROTECTION.....	7-1
7.3	LAW ENFORCEMENT	7-2
7.4	HEALTH	7-2
7.5	PUBLIC AREAS MAINTENANCE	7-2
7.6	TRANSPORTATION.....	7-2
7.7	SOLID WASTE AND RECYCLING.....	7-3
8	PUBLIC FACILITIES FINANCING.....	8-1
8.1	INTENT	8-1
8.2	FACILITY NEEDS AND COST ESTIMATES	8-1
8.3	OASP FEE PROGRAM AND OTHER FEES.....	8-4
8.4	FACILITY PHASING AND PROJECT CASH FLOW	8-8
8.5	IMPLEMENTATION PLAN	8-12
9	IMPLEMENTATION	9-1
9.1	ENVIRONMENTAL REVIEW	9-1
9.2	ANNEXATION	9-1
9.3	ZONING AND SUBDIVISION	9-1
9.4	PHASING.....	9-1
9.5	ARCHITECTURAL REVIEW	9-2
9.6	BUILDING PERMITS	9-2
9.7	TRANSFER OF DEVELOPMENT CREDIT/DENSITY CREDITS.....	9-2
9.8	CONSTRUCTION AND MAINTENANCE OF REQUIRED IMPROVEMENTS	9-5
9.9	AMENDMENT TO THE SPECIFIC PLAN.....	9-5

FIGURES

1.1	Site Location Map.....	1-3
1.2	Existing Site Conditions.....	1-7
1.3	Specific Plan	1-9
2.1	Wetland/Creek Impacts and Mitigation Plan	2-5
2.2	Creek Setback Standards.....	2-7
2.3	Creek Corridor Enhancement.....	2-7
2.4	Open Space and Parks Plan.....	2-11
2.5	Neighborhood Park Plan	2-13
2.6	Linear Park Cross-Section.....	2-14
2.7	Linear Park Details.....	2-14
2.8	Separated Pedestrian Path with Landscape Buffer.....	2-16
3.1	Potential Orcutt Area School Sites.....	3-14
3.2	Community Commercial Mixed-Use Plan.....	3-15
3.3	Community Commercial Mixed-Use Concept Sketches.....	3-17
4.1	R-1 Site Design Concepts	4-8
4.2	R-2 Site Design Concepts	4-9
4.3	Parking Courts	4-10
4.4	Architectural Design Concepts (R-1 and R-2 Zones)	4-11
4.5	R-2 Residential Design Concepts.....	4-12
4.6	Suggested Typical Site Lighting	4-17
4.7	Railroad Noise Reduction	4-21
4.8	Orcutt Road/Tank Farm Road Noise Setbacks.....	4-21
5.1	Circulation Plan	5-3
5.2	Orcutt Road from Laurel Lane to Johnson Avenue (A/B)	5-5
5.3	Orcutt Road from Johnson Ave. to Hansen Lane (A/B)	5-6
5.4	Orcutt Road from Hansen Lane to Tank Farm Road	5-7
5.5	North Side of Tank Farm Road from Railroad to Brookpine Drive.....	5-7
5.6	Collector: 'A' Street, 'D' Street, and a Portion of 'B' and 'C' Streets.....	5-10
5.7	Collector: 'B' Street Fronting Neighborhood Park	5-10
5.8	Traffic Calming Features on 'B' Street.....	5-11
5.9	Collector: 'C' Street from traffic circle to bridge.....	5-12
5.10	Collector: Bullock Lane	5-12
5.11	Local Streets in Residential Areas	5-14
5.12	Collector: 'C' Street through eastern Residential Area.....	5-14
5.13	Bicycle and Pedestrian Paths	5-16
6.1	Water Supply Plan	6-3
6.2	Wastewater Plan.....	6-5
6.3	Drainage Plan.....	6-9
6.4	Regional Detention Basin	6-11
6.5	Regional Detention Basin Cross Sections.....	6-13

9.1 Phasing Plan..... 9-3

TABLES

1.1 Land Use Summary..... 1-6

3.1 Residential Development Standards 3-4

3.2 Community Commercial Uses..... 3-5

4.1 Recommended Street Trees 4-15

8.1 Infrastructure Cost Estimate Summary 8-2

8.2 Detailed Infrastructure Cost Estimates 8-3

8.3 Benefit Unit Classifications and Cost Per Unit..... 8-5

8.4 City-Wide Impact Fees by Development Type 8-6

8.5 Specific Plan and EIR Fee 8-6

8.6 Project Specific Plus City-Wide and Other Impact Fees 8-7

8.7 Annual Absorption Assumptions..... 8-8

8.8 Cumulative Absorption Assumptions 8-8

8.9 Facility Phasing Assumptions..... 8-9

8.10 Annual Facility Cost Phasing..... 8-10

8.11 Project Specific Revenue Vs. Costs..... 8-11

APPENDICES

APPENDIX A OWNERSHIP AND DEVELOPMENT POTENTIAL/REQUIREMENTS &
INDIVIDUAL PROPERTY INFORMATION

APPENDIX B RESIDENTIAL GROWTH MANAGEMENT PHASING SCHEDULE

APPENDIX C REQUIRED MITIGATION MEASURES

APPENDIX D RECOMMENDED PLANT LIST

APPENDIX E CULTURAL RESOURCES BACKGROUND INFORMATION

1 INTRODUCTION

1.1 SCOPE OF SPECIFIC PLAN FOR ORCUTT PLAN AREA

The Orcutt Plan Area, located southeast of the City of San Luis Obispo (City), is designated as an expansion area within the urban reserve line in the City's General Plan. The General Plan requires that a Specific Plan for the entire Orcutt Area be adopted prior to annexation of any portion of the Plan Area. The Orcutt Area Specific Plan provides a comprehensive land use program to guide future public and private development in the planning area in conformance with the requirements set forth in the California Government Code Sections 65450 through 65457.

The Specific Plan provides a bridge between the City's General Plan and detailed plans such as development plans and subdivisions. It directs all facets of future development within the Orcutt Area including:

- Designation of land uses;
- Designation of required access & circulation elements;
- Location and sizing of infrastructure;
- Phasing of development;
- Financing methods for public improvements; and
- Establishing standards of development.

1.2 PLAN GOALS AND CONSISTENCY WITH THE GENERAL PLAN

The Orcutt Area Specific Plan is designed to meet the goals established in the City's General Plan by providing a framework for future development of the Orcutt Expansion Area. The Specific Plan is consistent with, and serves as an extension of, the City's General Plan. The policies and standards in the Orcutt Area Specific Plan will take precedence over more general policies and standards during the review of public and private development projects within the Specific Plan Area. In situations where policies or standards relating to a particular aspect of development have not been provided in the Specific Plan, the existing policies and standards of the City's General Plan and Zoning Ordinance will apply. In addition, the City's Residential Growth Management/Phasing Schedule, included as **Appendix B** and amended from time to time, will guide the rate of development within the Specific Plan area.

The overall goals of the Specific Plan are to:

- Develop a new residential neighborhood to meet the City's housing needs;
- Provide a variety of housing types and costs to meet the needs of renters and buyers with a variety of income-levels, including affordable housing for residents with low and very-low income levels;
- Protect and enhance Righetti Hill, creek/wetland habitats, and visual resources in open space areas;
- Provide a variety of park and recreational facilities for residents of the Orcutt Area;
- Phase the proposed development so that public facilities are developed concurrently with each new phase in a rational and cost effective fashion;
- Encourage the use of bicycles and walking within the Plan Area by incorporating bicycle/pedestrian paths and lanes along the roads and through the parks and open space areas;
- Protect the new residents from railroad noise by including a buffer between the railroad and the new residential areas. The buffer area will include a regional detention system consisting of floodable terraces and will provide recreational opportunities with a landscaped bicycle/pedestrian path system; and to

ORCUTT AREA SPECIFIC PLAN

1 Introduction

- Identify a visual and noise setback for new residents on Orcutt Road and Tank Farm Road by incorporating a landscape buffer with a pedestrian path along these road frontages.
- Provide goals and identify mitigation and related Environmental Impact Studies.

This Specific Plan amends the General Plan by extending the urban reserve line to enable the addition of approximately 30 residential lots designed to be compatible with the rural residential lots on the east side of Orcutt Road between Calle Crotala and Hansen Lane. Righetti Hill will be annexed into the City, but will remain outside of the URL because it is designated in the Specific Plan as Open Space.

1.3 SPECIFIC PLAN FORMAT

The Specific Plan provides discussion of the Plan Area, its anticipated future development, and the goals, policies, programs, and standards that will guide aspects of development in the Orcutt Area. The issues addressed in this plan are conservation, open space and recreation; land use and community design; circulation; public utilities and services; financing; implementation; and special considerations for individual owners in the Orcutt Plan Area. In the following chapters, goals for specific issues are followed by policies specific to the Orcutt Area. These policies will be implemented through various programs presented in the Specific Plan. Standards will define actions or requirements that must be fulfilled by new development.

1.4 LOCATION OF ORCUTT PLAN AREA

The Orcutt Plan Area encompasses 230.85 acres in the County of San Luis Obispo (County) immediately southeast of the City limits. The Orcutt Plan Area is bounded by Tank Farm Road to the south; Orcutt Road to the east and north; and the Union Pacific Railroad (UPRR) to the west (refer to **Figure 1.1**, Site Location Map). Righetti Hill is situated in the southern portion of the Plan Area. There are currently 13 property owners within the Plan Area with 21 parcels, one of which has already been annexed into the City. All of the property owners have had the opportunity to participate in this Specific Plan design through public workshops and meetings over a period of time starting in 1998.

1.5 PLANNING AREA CHARACTER

The Natural Setting

The 230.85-acre “Orcutt Area” is characterized by flat to rolling grasslands which rise to a steep, rocky hill at the southeastern corner of the area known as Righetti Hill. Nestled at the base of the Santa Lucia foothills, the Orcutt Area provides a variety of scenic resources for residents of the area and travelers along Orcutt Road and Tank Farm Road, including views of Righetti Hill, Islay Hill, and the Santa Lucia foothills. With a peak elevation of 563 feet and numerous rock outcroppings, Righetti Hill is a natural landmark listed in the City’s General Plan as one of the defining *Morros*. The majority of the Orcutt Area has unobstructed views of the Santa Lucia foothills located to the east of the Plan Area. In the General Plan, Orcutt Road and Tank Farm Road are designated as roads of moderate scenic value and one section of Tank Farm Road adjacent to the Orcutt Area is designated as a road of high scenic value. The Specific Plan is designed to preserve and enhance these scenic resources.

The Orcutt Area includes natural features such as creeks and wetlands, a variety of plant communities, hillside (Righetti Hill), and visual resources. **Figure 1.2** shows mapping of natural resources and other on-site conditions. The natural landscape includes plant communities such as annual grassland, riparian woodland, wetlands and central coast scrub. These habitats support a variety of wildlife. Riparian areas are primarily perennial creeks and the wetlands adjacent to creeks and small isolated agricultural wetland seeps on Righetti Hill. The Orcutt Plan Area’s historical uses of ranching and farming activities altered much of the native habitat although viable native plant communities and riparian areas remain.

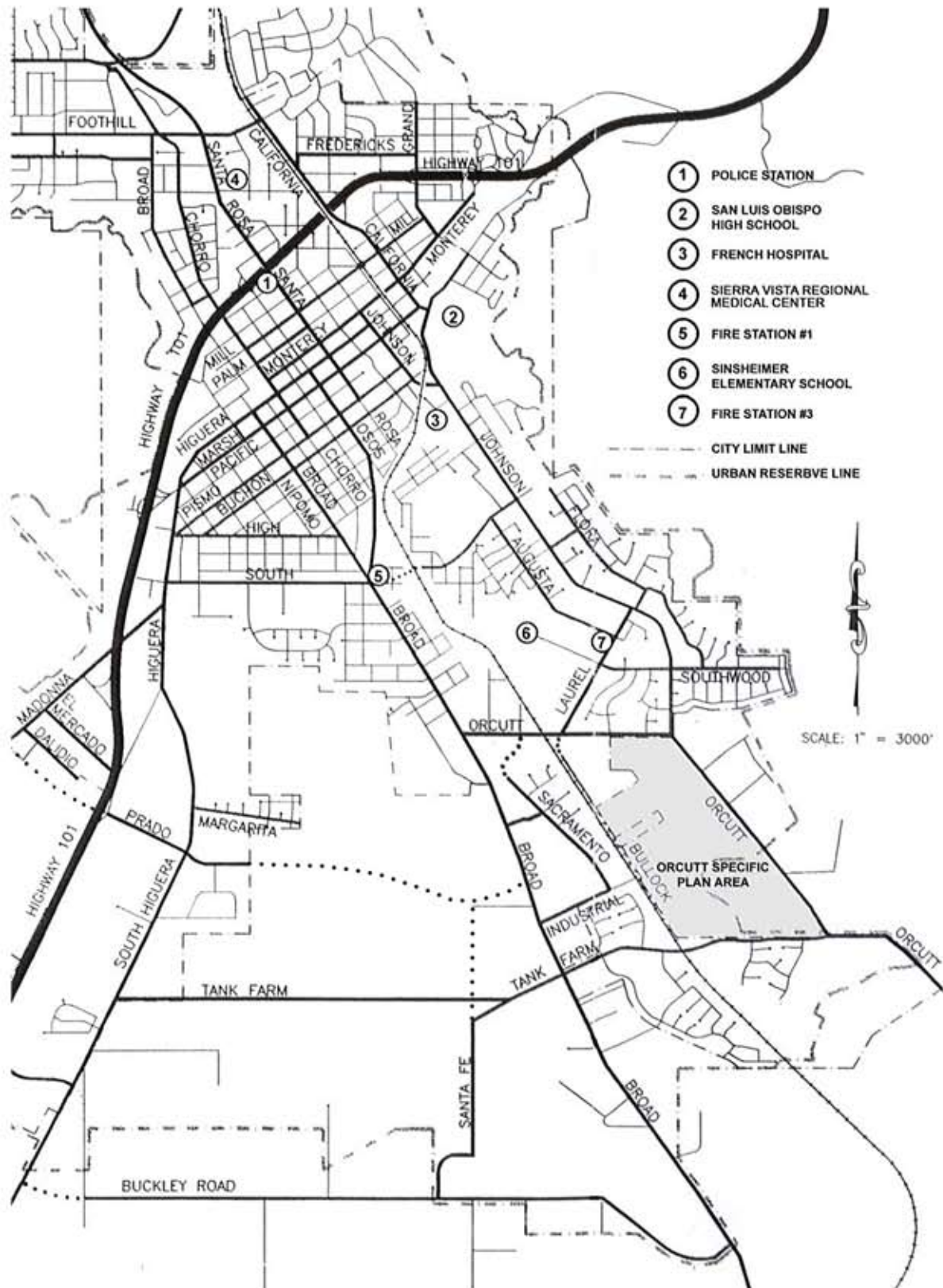


FIGURE 1.1 SITE LOCATION MAP

ORCUTT AREA SPECIFIC PLAN

1 Introduction

Settings and Uses in the Area

Pre-historic and Historic Settings

The project area lies within the historic territory of the Native American Indian group known as the Chumash. The archeological record indicates that sedentary populations occupied the coastal regions of California more than 9,000 years ago with the peak of their development occurring 800 to 150 years before the present time. The Chumash way of life changed forever with the Spanish colonization of California. By the end of the Mission Period in 1834, the Chumash population had been decimated by disease and declining birthrates. A more complete development of the Chumash is provided in Appendix F: Cultural Resources Background. The 2004 archaeological survey, based upon information visible at that time indicated that the site was mostly utilized for hunting and gathering and was not a major village site.

The general area after settlement by emigrants of European descent consisted of cattle grazing. The Orcutt Area during the Spanish and Mexican eras (1772 through 1848 with the passing of California from Mexico to the United States) was relatively undeveloped. There were adjacent lands dedicated to the Rancho Pecho Y Islay and the Rodriguez Adobe. This latter structure while not important enough to be declared a national landmark, has been incorporated into the Arbors Development to the south of the Orcutt area. The area was partially utilized for grazing and there is written record of at least one Mission adobe residence located at the outskirts of what later became the City of San Luis Obispo. Based upon early histories the adobe appears to have been located in the general area of Orcutt Road and Bullock lane near the intersection Laurel Lane.

The first private ranch in the area was owned by the Peter McMillan family which extended all the way to present day Broad Street (McMillan Lane was named after them). The next known reference is to Major Jackson who decided to stay in San Luis Obispo "... and Walter Murray located him on 160 acres where a big old adobe house stood, that had been owned by the Mission. Later he bought forty acres more. This land he sold in 1875 to J.H. Orcutt, and it was known for forty years as Laurel Ranch, or the J.H. Orcutt ranch." (History of San Luis Obispo County by Annie L. Morrison - Pages 85, 86, 87 and 95.) Jacob Orcutt expanded his landholdings up to 500 acres which apparently covered most of the OASP area (230 acres) as well as much of the adjacent area around the present day Laurel Lane. He and his wife maintained a dairy, developed orchards, and planted many of the eucalyptus trees visible today along the numerous small creeks in the area.

Land Use

The Specific Plan area's current land uses include a few scattered single-family residences on large parcels, primarily in the western and northeastern sections of the site, and agriculture-related uses, mostly cattle grazing on the southern and eastern portions of the Plan Area. The properties in the Orcutt Plan Area are in the County and are designated by the County's General Plan Land Use Element as Residential Single Family and Agricultural lands. The City's General Plan designates the area as an annexation area and the City's Land Use Element shows the Orcutt Area as Residential Neighborhood and Open Space. Prior to the development of urban uses, the general area was utilized for grazing or for dry-farming for the purposed of producing hay. The shortage of water limited development of irrigated crops. Other uses on properties in the Orcutt area include single-family homes, manufactured housings and commercial storage.

The Specific Plan area borders the Union Pacific Railroad tracks to the west, residential subdivisions within the Edna-Isly area to the south, rural residential development in the County to the east, and residential development to the north, including three existing mobile home/manufactured housing parks. The Specific Plan includes provisions that would allow for expansion of the Willow Creek Mobile Home Park into the Orcutt Area, which is one of many plan features that would help integrate the Orcutt Area into the surrounding community.

The existing land area contains two constructed features that may affect public safety in the area. First is the PG&E high voltage transmission line which runs east and west in an easement across the site connecting to the substation at the intersection of Orcutt Road and Johnson Avenue. This line will generate some level of electromagnetic force (EMF). As with electrical fields, the strength of the magnetic field decreases as the distance from the source increases. Magnetic fields on the ground measured under electrical transmission lines are usually smaller than the magnetic fields associated with electrical appliances. While there has been concern raised about the health hazards of EMF, there are no recent scientific studies which indicate that this would be true for the conditions within the Orcutt Area Specific Plan. In June of 1999, the National Institute of Environmental Health Science completed a research program which concluded that the probability of exposure to EMF being a health hazard was small.

The second feature of concern is the location of the Union Pacific Railroad which lies along the west boundary of the Specific Plan area. Trains (and trucks) commonly carry a variety of hazardous materials, including gasoline and various crude oil derivatives, and other chemicals known to cause human health problems. When properly contained these substances present no hazard to the community. However, under accident conditions such as a derailment, such materials may be released either in liquid or gas form. In the year 2000 nationwide, there were rail accidents that caused enough damage to 75 rail cars that releases of hazardous materials were the result. There was one fatality as a direct result of an accident and 82 injuries. The EIR evaluation concludes that “ Although standard accident and hazardous materials recovery procedures are enforced by the state and followed by private transportation companies, the site is at relatively high risk because of its location along the rail corridor.” There is also concern about trespasser casualties (deaths or injuries) since there are currently no effective barriers to trespassers crossing the tracks. This Specific Plan includes features to mitigate these safety concerns, including setbacks from the Railroad Right-of-Way and fencing to prevent pedestrians from crossing the tracks.

1.6 SPECIFIC PLAN FEATURES

The major features of the Specific Plan include hillside and creek open space areas with bike and pedestrian paths, and a public park with a potential school site in the center of the Plan Area surrounded by residential neighborhoods. A modest community commercial retail and office zone is also proposed. The Orcutt Area Plan is designed to protect the natural resources of the site through generous reservations of open space including the upper slopes of Righetti Hill, wetlands, creeks, and riparian corridors. Residential neighborhoods will be developed around the natural features of the landscape, incorporating the creeks, riparian areas, and hills into the site plan while respecting the sensitivity of the resources. A centrally located park, surrounded by creek open space with trails, unites the residential areas to create a cohesive neighborhood atmosphere and provides a large common area for recreation. A community commercial mixed-use area near the park further enhances the social interactions focused around the park. A linear park with pedestrian/bicycle paths, located along the western boundary of the Plan Area, connects to the City’s bicycle path system along the UPRR right-of-way. The pedestrian and bicycle paths provide internal connectivity of the neighborhoods with the Plan Area as well as connectivity of the Plan Area to existing neighborhoods and commercial areas.

The Specific Plan calls for a balanced mix of housing types including single-family and multi-family residential areas, and two sites for public or low-income housing developments. The mixture of housing will provide a range of housing densities and types appropriate for renters and buyers with various income-levels and lifestyles. A network of biking and walking paths linking the residential areas, the centrally located park, and the mixed-use/neighborhood commercial area will help facilitate social interactions between neighbors. The circulation plan for the new development is designed to provide optimum internal access to the residential areas while preserving quiet neighborhood conditions and minimizing through trips.

ORCUTT AREA SPECIFIC PLAN

1 Introduction

Development of the Orcutt Area will be phased to ensure that necessary public services and facilities are available to serve the approximately 2,000 new residents. The build-out potential for the Specific Plan Area is a range of 892-979 housing units with the fee program based on the average of 937 units. **Table 1.1** presents a summary of the proposed land uses in the Orcutt Area, and **Figure 1.3** illustrates the Specific Plan with proposed open space, parks, development areas, and roads.

TABLE 1.1 LAND USE SUMMARY

Land Use	Zoning	Acres	Density	Total Units (Estimated)	% of Orcutt Area
RESIDENTIAL					
Low Density Residential Detached single family, 4,500 - 15,000 sf. Lots	R-1-SP	51.0	Up to 7 du/acre ²	264	23.1
Medium Density Residential Detached/attached single family w/zero lot line; duplex units ¹ Minimum lot size of 3,000 sf.	R-2-SP	32.0	Up to 12 du/acre ²	276	13.5
Medium-High Density Residential Multi-plex units; mobile homes and multi- family apartments ¹	R-3-SP	22.3	Up to 18 du/acre ²	336	9.0
High Density Residential Multi-family apartments ¹	R-4-SP	5.5	Up to 24 du/acre ²	103	2.4
Subtotal		110.8		979³	48.0
COMMERCIAL					
Community Commercial/ Mixed Use	CC-MU	2.75 ⁴			1.2
OPEN SPACE AND RECREATION					
Open Space	C/OS-SP	79.3			33.3
Parks					
<i>Neighborhood Park (ball fields, ball courts, playgrounds)⁵</i>	P-F-SP	12.0			4.7
<i>Trail Junction Park/Pocket Parks</i>	P-F-SP	4.3			1.6
<i>Potential joint use facility as part of future elementary school</i>	in residential zones	4.0			
Total Potential Parks		20.3			6.3
Central Detention Area/Floodable Terrace		4.8			2.0
Creeks		6.7			2.9
PUBLIC FACILITIES					
Roads Arterials, Collectors and major Local		14.6			6.3
TOTAL		230.9		979⁵	100.0

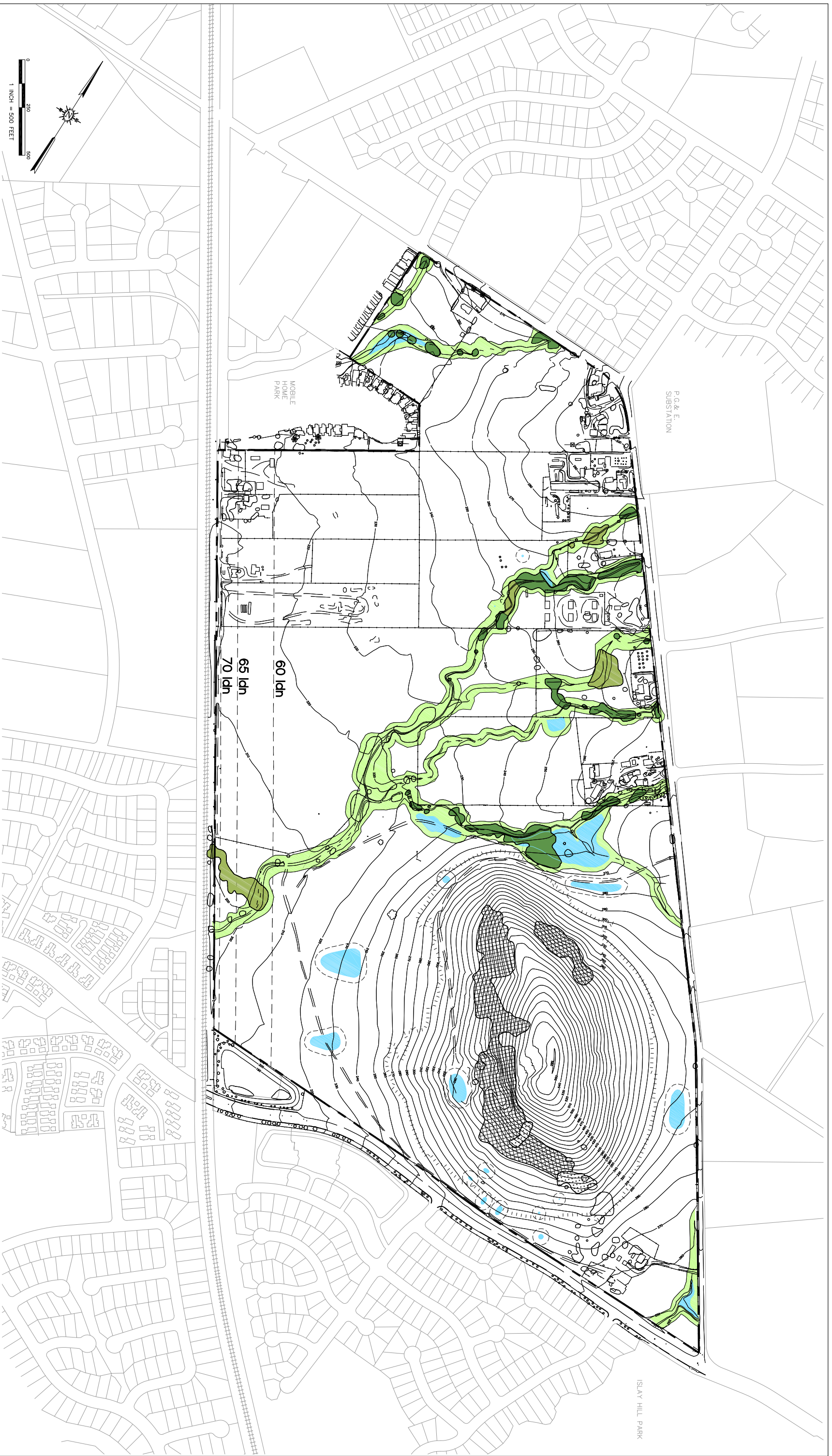
¹ These types of housing reflect examples of housing types within each residential category.

² This range reflects the maximum densities for residential development.

³ This figure represents max. development potential before affordable housing density bonuses.

⁴ This acreage is for CCMU and is expected to support 8,000 SF of retail and 8,500 SF of office space. The balance of the area will be devoted to residential in a mixed-use configuration.

⁵ This plan provides 12 acres total in the central neighborhood park. Dedication will be from two to three owners.



ORCUTT EXPANSION AREA
S P E C I F I C P L A N

LEGEND










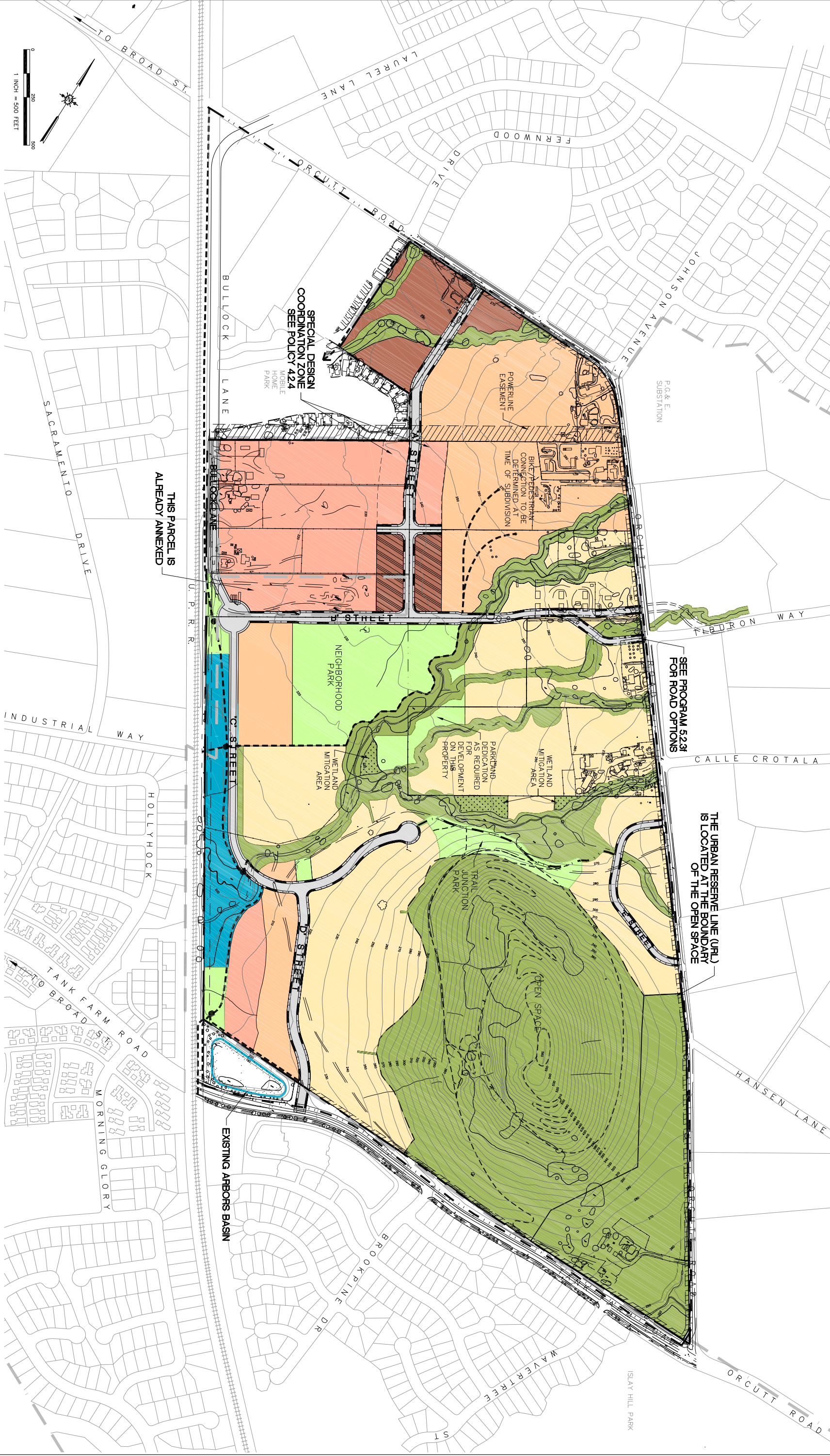
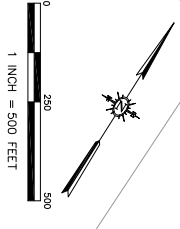
-  WETLAND
 -  CREEK CORRIDOR
20' FROM BANK OR RIPARIAN
 -  NATIVE RIPARIAN HABITAT
 -  CACTUS
 -  CENTRAL COAST SCRUB
 -  EUCALYPTUS
 -  30' WETLAND BUFFER
 -  NOISE CONTOURS
 -  30% SLOPE LIMITS
- NOTE: RECONNAISSANCE LEVEL SITE
ASSESSMENT ONLY - NOT A
SURVEYED WETLAND DELINEATION.
(WETLANDS MAY BE OTHER THAN SHOWN)

FIGURE 12
EXISTING SITE CONDITIONS



AUGUST 2009



ORCUTT EXPANSION AREA
SPECIFIC PLAN

- LEGEND - PROPOSED LAND USE**
- R-1: LOW DENSITY RESIDENTIAL
 - R-2: MEDIUM DENSITY RESIDENTIAL
 - R-3: MEDIUM-HIGH DENSITY RESIDENTIAL
 - R-4: HIGH DENSITY RESIDENTIAL
 - MIXED USE/ NEIGHBORHOOD COMMERCIAL
 - PARKLAND
 - OPEN SPACE
 - WETLAND MITIGATION AREA
 - DETENTION BASIN
 - EXISTING ARBORS BASIN
 - URBAN RESERVE LINE (URL)
 - CITY LIMIT
 - ORCUTT AREA BOUNDARY
 - CLASS I BICYCLE PATH
 - CLASS II BICYCLE PATH
 - RECREATION TRAIL

FIGURE 13
SPECIFIC PLAN

2 CONSERVATION, OPEN SPACE, AND RECREATION

2.1 INTENT

Key goals of this specific plan are to protect and enhance Righetti Hill, creek and wetland habitats, visual resources in the open space area and to provide a variety of park and recreational facilities for residents of the Orcutt Area. This section of the specific plan sets the framework for protecting the open space and sensitive resources and for providing recreational opportunities in the plan area. The Plan includes dedications to the City for recreational facilities and open space areas, and provides for the preservation of approximately 33% of the gross plan area as Open Space (80 acres).

The City's Conservation and Open Space Element defines open space as:

...any land or water area that remains in a predominantly natural or undeveloped state and which is generally free of structures, inside or outside City limits, within the San Luis Obispo Planning Area ... such lands protect and preserve the community's natural and historical resources, define the urban boundary, and provide visual and physical relief from urban development. (Conservation and Open Space Element, Chapter 8)

Open space resources in the Orcutt area include creeks, wetlands, riparian areas, and plant and animal habitat; agricultural lands; historic and archaeological resources; and scenic resources. In this section of the plan, natural resources in the Orcutt area consist of Righetti Hill, creeks, wetlands, and native and non-native flora and fauna.

2.2 BIOLOGICAL RESOURCES

The topography of the Specific Plan Area adds to the identity and beauty of this sector of the City. The Plan Area's most notable natural feature is Righetti Hill - one of several "Morros" which punctuate the community of San Luis Obispo - a unique and rare landform.

Limited site surveys by biological consultants in recent years reveal that the plan area is comprised of native and non-native habitat types. Native habitat types include riparian woodland, wetland, and coastal scrub. Nine drainage or riparian corridors traverse the Plan Area.

The drainages flow in a primarily northeast to southwest direction across the project site, and six of these drainages converge in the middle of the site. Water leaves the site as one drainage through a single culvert in the southwestern portion of the area. All of the on-site drainages are ultimately tributary to San Luis Obispo Creek.

Non-native annual grassland habitat covers the majority of the site and occasional stands of eucalyptus trees, a non-native species, are present. The success of the non-native grasslands is largely attributable to the long-term use of the site for cattle grazing.

This section includes policies and programs that will facilitate and protect the long-term establishment of native habitat types in the Plan Area.

Creek corridors sustain wildlife, offer scenic environments for area residents, and provide a flood control function. Various creek corridors (creek channels and surrounding creek buffers), which cross the Orcutt Area, will be protected as open space and some portions of the creek corridors will be incorporated into park design. Creek corridor locations as identified in this Specific Plan are based on preliminary site assessments. Actual location of creek corridors and related setback areas may vary. Exact location of creek corridors and setbacks shall be determined at the time that development is proposed.

Goal 2.2a: Preservation and enhancement of creek, riparian, and wetland habitats in the Plan Area to insure habitat value and limited recreational opportunities.

ORCUTT AREA SPECIFIC PLAN

2 Conservation, Open Space and Recreation

Goal 2.2b: Circulation and development within the Specific Plan Area that minimizes impacts to creeks and wetlands.

Policy 2.2.1: Designate and set aside approximately 19 acres of creeks, wetlands, riparian vegetation and required buffers as Open Space. Those portions of creeks, wetlands, riparian vegetation and buffers adjacent to park areas will be incorporated into park area, dedicated to the City, but not included in the acreage calculations (Figure 2.1).

Policy 2.2.2: Avoid and minimize impacts to wetland, creek, and riparian areas.

Program 2.2.2a: All development is subject to a 20-foot creek setback. The setback shall be measured from the top of bank or from the edge of the predominant pattern of riparian vegetation, whichever is farther from the creek's flowline, except for proposed road crossings (**Figure 2.2**). The location of top of bank and riparian vegetation shall be shown on all proposed project plans, subject to the review and approval of the City's Natural Resources Manager.

Program 2.2.2b: All development shall be set back at least 30 feet from all on-site wetland habitats on the project site. The location of wetland vegetation shall be shown on all proposed project plans, subject to the review and approval of the City's Natural Resources Manager. Wetland areas shall be identified through a formal wetland delineation study conducted by a qualified biologist utilizing methodologies accepted by the U.S. Army Corps and State of California.

Program 2.2.2c: Development that abuts riparian and wetland mitigation areas shall also be setback at least 20 feet and include appropriately-sized fencing and/or native plant landscaping that deters human entry to the mitigation area. Fencing shall allow views into the corridors.

Policy 2.2.3 Impacts to wetland, creek, and riparian areas shall be mitigated consistent with the City's Conservation and Open Space Element (COSE). Mitigation shall include creation of twice the area of habitat lost (2:1 ratio), of equal quality and similar kind, within Orcutt Specific Plan Area. Implementation of measures included in Appendix C is also be required, to the approval of the Community Development Director.

Program 2.2.3a: Approximately 1.94 acres of wetland creation and 2.76 acres of riparian enhancement along the creek corridors, in the neighborhood park and in the linear park shall be implemented to improve the existing riparian habitat with no net loss to wetland and creek resources in the Orcutt Area (**Figures 2.1** and **2.3**). Modification or management of riparian areas for flood damage reduction, stream enhancement, and bank repair shall be consistent with the Section 11 guidelines of the City's Creek Drainage Design Manual.

Program 2.2.3b: Approximately 0.78 acres of isolated agricultural wetland seeps on the west side of Righetti Hill are also proposed to be filled to allow residential development.

Policy 2.2.4: Where crossings of major creeks are necessary, clear span bridges will be used to minimize impacts to the creeks. Minor creeks or wetland swales may be crossed using culverts.

Program 2.2.4a: Three bridged creek crossings are proposed to allow new roads within the Orcutt Area. These crossings will entail approximately 0.12 acres of fill of creek or wetland buffers.

Program 2.2.4b: All bridges, culverts, and modifications to the existing creek channels will comply with the City's Drainage Design Manual (DDM) and applicable City policies with consultation and approval from the Director of Public Works. Additional review and

permits may be required from the U.S. Army Corps of Engineers and California Department of Fish and Game. Project proponent will provide proof of consultation and copies of necessary permits to the City Community Development Director.

Policy 2.2.5: Recreational trails will be located outside the setback required by the City's General Plan and Zoning Ordinance to the greatest extent possible. Some trails will be located parallel to creeks. Trails may be placed in the outer perimeter of the creek setback when development of recreation trails or bike paths would otherwise cause significant loss of developable land for housing. City Council may approve such uses as part of a park design and with approval of subdivision maps.

Program 2.2.5a: Recreational access to creeks and wetlands will be discouraged by planting native plant species between trail and other recreation features and the wetland/riparian habitat.

Program 2.2.5b: Educational signage to provide information about wetland and creek habitats shall be installed on public trails and/or in public open space areas.

Policy 2.2.6: Design on-site drainage detention areas to support wetlands characteristics so that they may provide aesthetic, habitat and flood control benefits

Goal 2.2c: Preservation of unique plant and animal communities, particularly native California species.

Policy 2.2.7: Protect and enhance habitat through re-introduction of native California vegetation and removal of non-native species.

*Policy 2.2.8: Avoid or minimize impacts on sensitive natural communities, special-status plant species and habitats, non-listed special-status wildlife species, and listed wildlife through the implementation of measures included in **Appendix C** at the discretion of the Community Development Director. Listed species include those identified by the U.S. Fish and Wildlife Service, the California Department of Fish and Game, the California Native Plant Society and/ or the City of San Luis Obispo. Mitigation may include creation of twice the area of habitat lost (2:1 ratio), of equal quality and similar kind, within Orcutt Specific Plan Area.*

Righetti Hill

Preservation of Righetti Hill as open space is a critical part of this Specific Plan. The City's General Plan directs Orcutt Area properties to dedicate land or easements covering the Santa Lucia foothills and Righetti Hill as identified in the Conservation and Open Space Element. (General Plan LUE 1.12.5.C)

Goal 2.2d: Permanent protection of Righetti Hill through easements or land dedications.

Policy 2.2.9: The majority of Righetti Hill (approximately 48 acres) shall be preserved as natural Open Space through dedication to the City. This area includes the hill from the 320-foot elevation to the top of the hill, an abandoned rock quarry located on the west slope, and an existing unpaved access road. No development or private lots will encroach on this area.

Program 2.2.9a: The City will manage this area in accordance with City standards for Open Space areas. The City may manage hillside vegetation along the development limit line on the hill to avoid concentrations of chaparral or other plants that could support intensive fires. Fire safety will be achieved primarily by maintaining separation within the area that is eligible for development, between fire-prone natural vegetation

ORCUTT AREA SPECIFIC PLAN

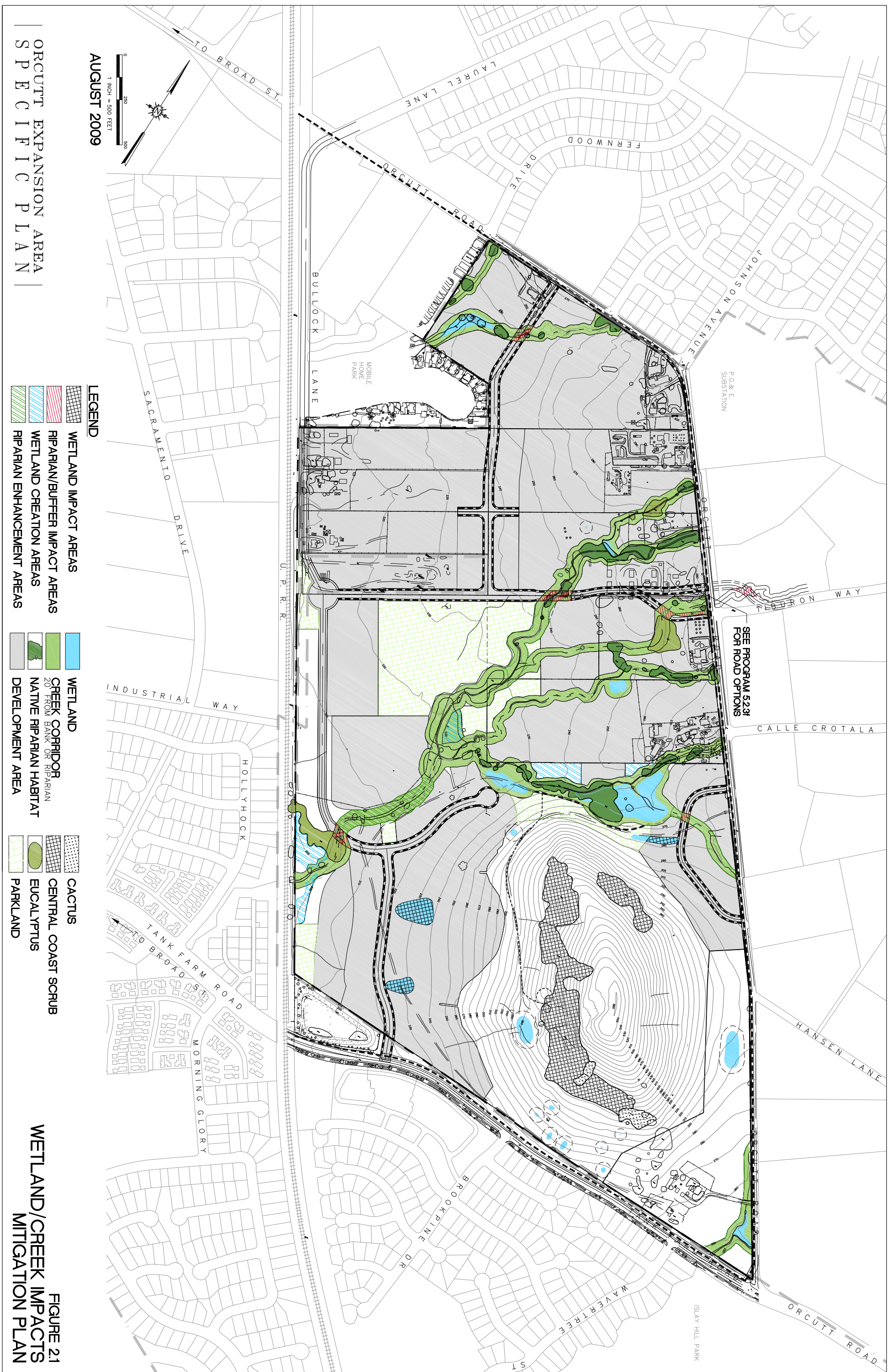
2 Conservation, Open Space and Recreation

and combustible structures, including wood fencing and sheds. Landscape plantings in the fire-safety setbacks will be low growing and not fire prone. To allow natural plant communities to regenerate, livestock grazing on the hill will be prohibited.

Program 2.2.9b: The City will provide and maintain public access to Righetti Hill, including the existing unpaved access road, and preserve the aesthetic values and biological resources on the hill. Prior to allowing public access to the top of the hill, the City will develop a management plan for the open space resource consistent with the Conservation and Open Space Element. The management plan will incorporate the archeological study required by EIR mitigation measure CR-1(b) (See Appendix C).

Policy 2.2.10: *The east flanks of the hill, with the Righetti family ranch home, shall be designated as Open Space. A conservation easement will be granted to the City for this 15.3-acre parcel and ownership and access will remain private. No further subdivision of this parcel will be permitted. Allowable uses on this parcel include agriculture, farmhouse and secondary dwelling, recreational horse ranch, and other passive or active recreational uses. The purpose of this easement is to allow the Righetti family home site to remain in private ownership while limiting development of the parcel.*

Program 2.2.10a: The land owner maintains the right to the existing number of structures onsite (two independent residential units, outbuildings, and related structures) and will manage the parcel in accordance with City standards for Open Space areas. The creek habitats in the southeast corner of this parcel will be enhanced with native plantings and the removal of non-native vegetation.



1 INCH = 500 FEET

 0 250 500

 AUGUST 2009

ORCUTT EXPANSION AREA
SPECIFIC PLAN

LEGEND

- | | | | | | |
|--|------------------------------|--|---|--|---------------------|
| | WETLAND IMPACT AREAS | | WETLAND | | CACTUS |
| | RIPARIAN/BUFFER IMPACT AREAS | | CREEK CORRIDOR
20' FROM BANK OR RIPARIAN | | CENTRAL COAST SCRUB |
| | WETLAND CREATION AREAS | | NATIVE RIPARIAN HABITAT | | EUCALYPTUS |
| | RIPARIAN ENHANCEMENT AREAS | | DEVELOPMENT AREA | | PARKLAND |

FIGURE 2.1
WETLAND/CREEK IMPACTS
MITIGATION PLAN

FIGURE 2.2 CREEK SETBACK STANDARDS

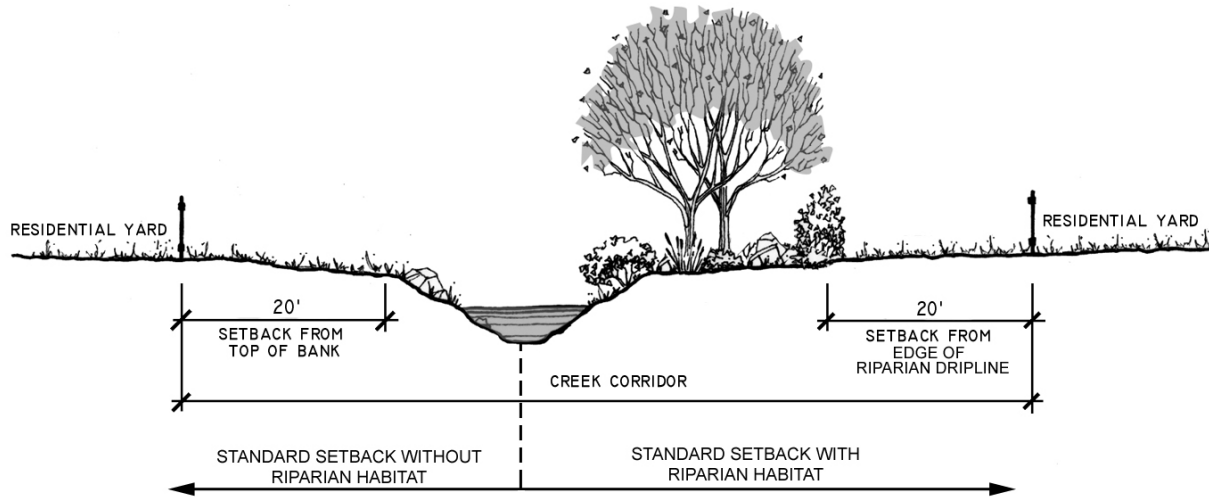
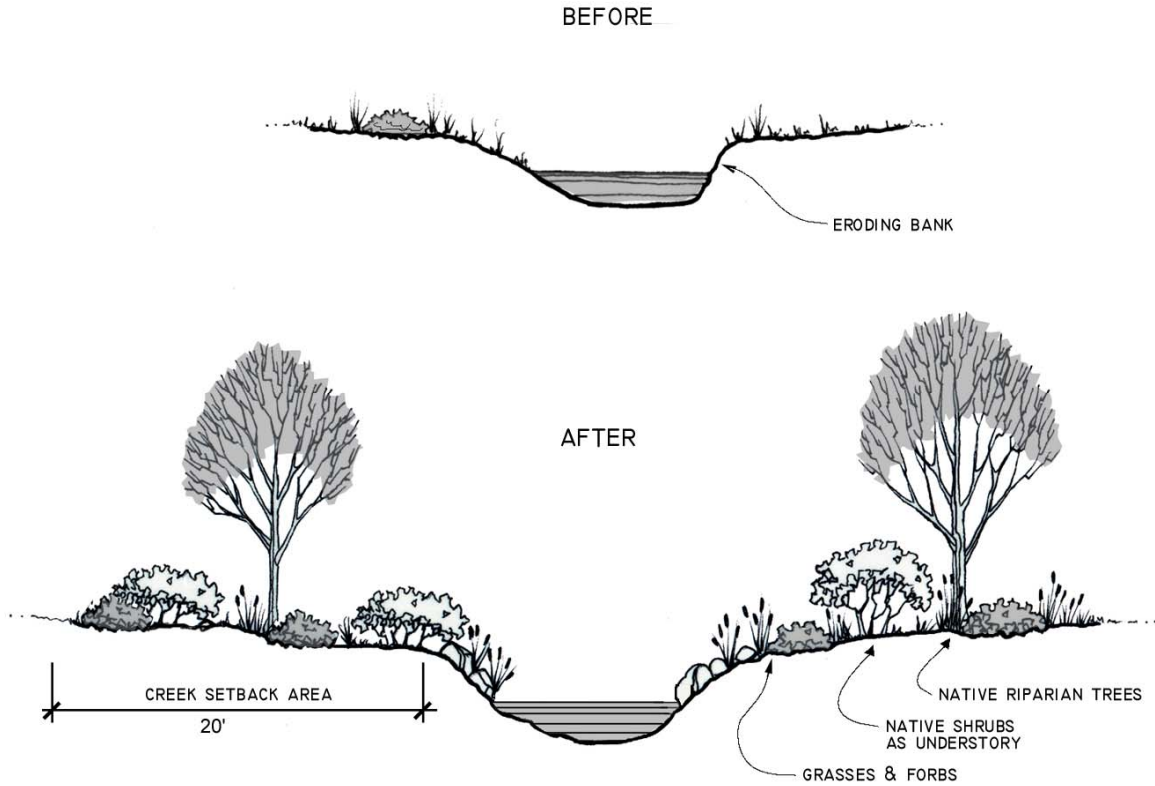


FIGURE 2.3 CREEK CORRIDOR ENHANCEMENT



2.3 RECREATION RESOURCES

Recreation resources include a neighborhood park, several pocket parks, a linear park at the base of Righetti Hill and bicycle/pedestrian paths throughout the Orcutt Area. The neighborhood park, located at the center of the Orcutt Area, will serve as a community gathering place for casual recreation and sporting events by providing a variety of active recreation facilities. The linear park will combine utility with pleasure by doubling as both a passive recreation/viewing area and stormwater management area.. The pocket parks provide green access points to the railroad bike path and passive recreation facilities such as seating for wildlife viewing.

Neighborhood and Linear Parks

The neighborhood park is considered an essential component of the OASP and a major amenity to the neighborhoods in the Orcutt Area. The location of the neighborhood park is designed to facilitate interaction between single-family and multi-family residential areas; it will be located close to the residential neighborhoods and serve as a natural focal point for the mixed-use area in the central portion of the Orcutt Area (**Figures 2.4 and 2.5**). As shown in **Figure 2.5**, the neighborhood park should incorporate the fringe of the adjacent riparian area into the setting of the park. The convergence of two channels of the upper fork of the East Branch of San Luis Obispo Creek is a valuable resource to the Orcutt Area. A portion of the creek channels are incorporated as an amenity of the park system shown on the Specific Plan.

Goal 2.3a: Opportunities for active and passive recreation through the creation of public parks.

Goal 2.3b: Adequate parkland to support the population of the neighborhood.

Policy 2.3.1: The City Parks and Recreation Element requires a minimum of 10 acres of parkland per 1,000 city residents.

Policy 2.3.2: Estimate the anticipated number of residents in the Orcutt Area based on the 2000 Census San Luis Obispo Average Household sizes used by the City as population generation rates of 2.58 residents per R-1 and R-2 units and 1.92 residents per R-3 and R-4 units.

Policy 2.3.3: The unit count used to calculate the required park acreage for the Specific Plan Area is based on assumed development of a reasonable maximum of allowed units on all properties in the Plan Area. Park acreage required as a result of development on the Garay property (0.9 acres) will be dedicated if and when entitlement requests for this property are submitted to the City.

Program 2.3.3a: Based on Policies 2.2.1, 2.2.2, and 2.2.3, and the anticipated unit development for the Orcutt Area shown in **Table 1-1**. The OASP identifies 16.30 acres of active and passive parkland, including a 12-acre neighborhood park, a .25-acre pocket park at the end of 'D' Street, 1.54 acres of linear parkland associated with the regional detention basin and a 2.5 acre 'trail-junction' park consisting of passive parkland. In addition to these 16.3 acres, the City will pursue the development of approximately 4 acres of parkland as part of a joint use facility with the San Luis Coastal Unified School District if a new school is developed within or near the Orcutt Area.

Program 2.3.3b: A 12-acre centrally located neighborhood park will be developed in the Orcutt Area. 10 acres shall be dedicated during Phase I development, and construction of the neighborhood park will occur as identified in the Public Facility Financing Plan (PFFP) in Chapter 8. Facilities may include a playground for young children, soccer and baseball fields, tennis courts, basketball courts, sand volleyball, picnic tables and

restrooms. Security lighting will be provided in the parking lots and at the restrooms but to avoid glare impacts to surrounding residences, major night lighting will not be used at the park or ball fields. An example of this park is shown in Figure 2.5: Neighborhood Park Plan. If development occurs on the Garay property, its required parkland dedication (0.9 acres) shall occur in such a way as to augment the central neighborhood park.

Program 2.3.3c: A 2.5 acre ‘trail-junction’ park will be developed along the northern boundary of Righetti Hill. Dedication of acreage and construction of the linear park will occur in as identified in the PFFP, Chapter 8.

The linear park will have multiple uses including paths for recreational bicycling and walking; picnic areas for viewing the Edna Valley and a starting point for hikers to access Righetti Hill.

Program 2.3.3d: 1.5 acres of park will be developed along the western portion of the Specific Plan Area adjacent to the stormwater detention basin and wetland areas. The two pocket parks will provide resting points and information areas in support of the Class I bicycle/pedestrian path. The bicycle/pedestrian path will connect to the existing Class I bicycle path along the UPRR right-of-way and to the neighborhood park, allowing for multiple recreational opportunities for residents and visitors to the park. Picnic tables, benches, viewpoints, and educational signage about the wetland habitat project will provide additional recreational and educational opportunities for users of the park (**Figures 2.6 and 2.7**). This park area will not have night lighting to avoid impacts to wildlife.

Program 2.3.3e: The remainder of the park acreage associated with Orcutt Area development, 4 acres, will be provided by a joint use facility to be developed with the San Luis Coastal Unified School District when a planned elementary school is developed in or near the Orcutt Area.

Policy 2.3.4 Provide property owners within the Orcutt Area that want to provide parkland on their properties an opportunity to do so through the subdivision review process.

Program 2.3.4a: When subdivisions are proposed in the future, property owners in the Orcutt Area may provide improved parkland on their property instead of paying the parkland fee identified in the OASP Fee Program (see Chapter 8), under the following circumstances:

- i. The Parks and Recreation Commission determines that the additional parkland serves a community-wide need that is not met through the provision of other parkland in the Orcutt Area.
- ii. The proposed parkland can be provided without reducing the number of residential units anticipated for the property, as illustrated by the range of Total Estimated Units shown in Table A-2 (Appendix A).
- iii. The provision of additional parkland will not impact the ability of Orcutt Area development to finance parkland acquisition costs identified in Chapter 8, Public Facilities Financing, because other funding sources insure the feasibility of financing this parkland acquisition. Other funding sources can include the following:
 - Grant funding for parkland acquisition.
 - Development in the Orcutt Area in excess of the 937 units used to estimate available fees for parkland acquisition in Chapter 8, making more money available for parkland acquisition than previously anticipated.

ORCUTT AREA SPECIFIC PLAN

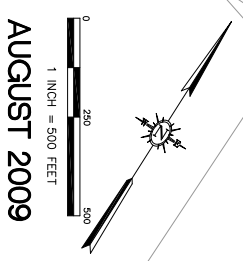
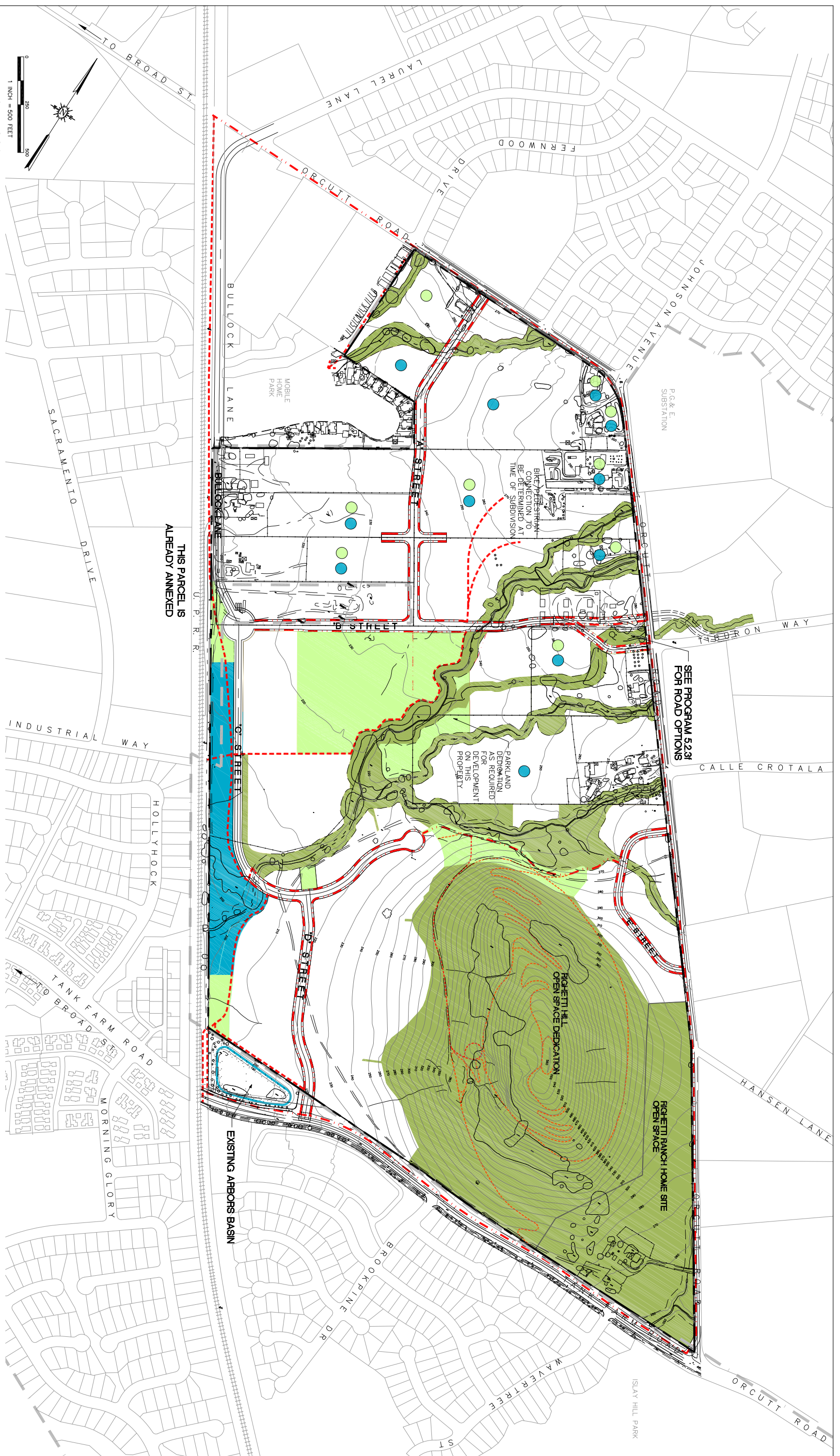
2 Conservation, Open Space and Recreation

- City funding of parkland acquisition to meet regional parkland needs, if approved by the City Council.

Bicycle/Pedestrian Paths

Both Class I and Class II bicycle/pedestrian paths are an essential component of the recreation facilities provided in the Orcutt Area, and an integral part of the Specific Plan circulation system. Goals, policies, and programs for bike paths are discussed in the Circulation chapter (Section 4.5).

In coordination with the City's Natural Resources Manager, a relatively extensive pedestrian trail system (projected to be more than 3000 linear feet) is proposed for Righetti Hill and will be developed by the City of San Luis Obispo, consistent with City Open Space standards.



ORCUTT EXPANSION AREA
SPECIFIC PLAN

- LEGEND**
- OPEN SPACE
 - PARKLAND
 - OPTION FOR LOCAL PARKLAND DEDICATION
 - DETENTION BASIN
 - EXISTING ARBORS BASIN
 - OPTION FOR LOCAL DETENTION BASIN
 - RECREATION TRAIL
 - CLASS I BIKE PATH
 - CLASS II BIKE LANE
 - RIGHETTI HILL TRAIL

OPEN SPACE AND PARKS PLAN

FIGURE 2.4

FIGURE 2.5 NEIGHBORHOOD PARK PLAN



FIGURE 2.6 LINEAR PARK CROSS-SECTION

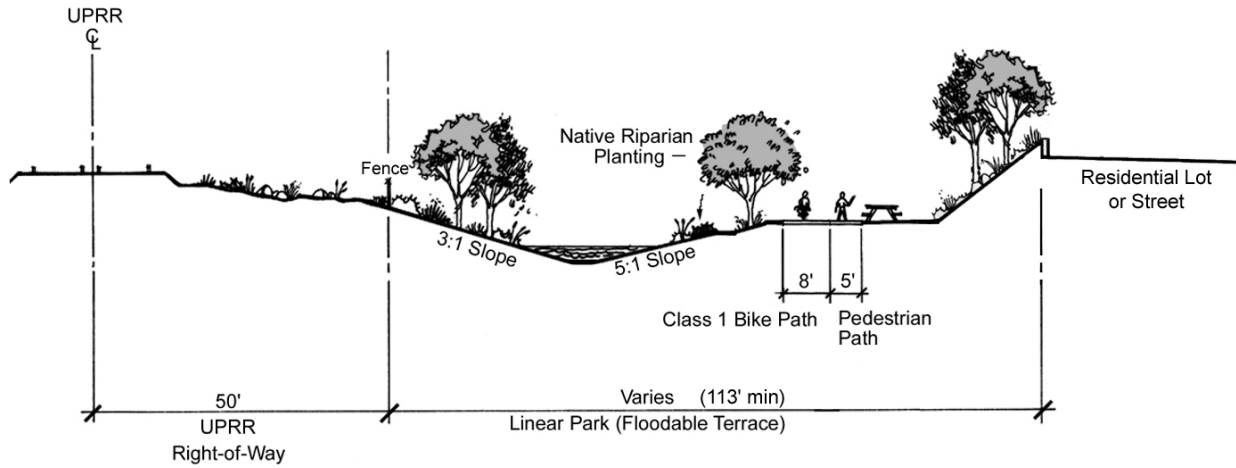


FIGURE 2.7 LINEAR PARK DETAILS



2.4 SCENIC RESOURCES

The Specific Plan area is bounded by Tank Farm Road to the south, Orcutt Road to the east and north, and the Union Pacific Railroad (UPRR) to the west. The Orcutt Area is characterized by flat to rolling grasslands which rise to a Righetti Hill at the southeastern corner. Nestled at the base of the Santa Lucia Mountains, the Orcutt Area provides a variety of scenic resources for residents and travelers along Orcutt Road, including views of Righetti Hill, Islay Hill, and the Santa Lucia foothills. Righetti Hill is a natural landmark, visible from most of the southern portion of the City, and is listed in the City's General Plan as one of the defining Morros. Portions of Orcutt and Tank Farm roads adjacent to the Specific Plan Area are designated as roads of high and moderate scenic value in the City's General Plan. Orcutt Road, as a designated City Gateway, provides a scenic backdrop to those traveling into the City from the southeast.

Goal 2.4: Attractive scenic corridors and city gateway.

Policy 2.4.1: Minimize impacts to public views from scenic roadways.

Program 2.4.1a: A minimum 20 foot wide landscaped setback will be included along Orcutt Road and Tank Farm Road with a pedestrian walkway except where creeks intersect Orcutt Road. **(Figure 2.8)** In these locations, landscaping will be reduced to minimize creek impacts. Where existing dwellings front Orcutt Road, construction of landscaping and walkways will occur at the time of development of the property to avoid impacts to existing residences. The landscaped setbacks and walkways will provide attractive views for motorists on Orcutt Road, safe pedestrian access, and noise buffering for new residents along Orcutt Road.

Program 2.4.1b: Consistent with the Circulation and Conservation and Open Space Elements of the City's General Plan, designate parcels adjacent to the following scenic roadways as 'sensitive' and require architectural review of all proposed development on 'sensitive' parcels to insure the preservation of views, consistent with the General Plan, while allowing for reasonable development of building sites.

Road of High Scenic Value:

- Tank Farm Road from the railroad tracks to Brookpine Drive

Road of Moderate Scenic Value:

- Tank Farm Road from Brookpine Drive to Orcutt Road.
- Orcutt Road along the northern boundary of the site

Road of High or Moderate Scenic Value Outside of the City Limits

- Orcutt Road along the eastern boundary of the site

City Gateway:

- Orcutt Road where it is within the Greenbelt.

Program 2.4.1c: During the development review process, the Architectural Review Commission (ARC) shall approve landscaping plans for the City's landscaped setback areas along designated scenic corridors. Trees shall be clustered to allow some backdrop view corridors. At the time of occupancy, planted landscaping in the landscaped setback areas, screening foreground views shall screen views of new structures at 30% of their designed intent. Screening ratio and landscape vegetation health shall be achieved under the supervision of a qualified arborist or landscape architect, in consultation with the City Arborist.

ORCUTT AREA SPECIFIC PLAN

2 Conservation, Open Space and Recreation

- Program 2.4.1d: Buildings on ‘sensitive’ parcels adjacent to Orcutt Road and Tank Farm Road shall not include a second story unless the two-story portion of the building is set back from the residential property line by at least 50 feet to maintain views of Righetti Hill and other important visual resources.
- Program 2.4.1e: During the Subdivision Review process, the Planning Commission shall consider the overall size, width, depth, and orientation of lots within ‘sensitive’ parcels adjacent to Tank Farm Road east of Brookpine and Orcutt Road along the northern boundary of the plan area to insure that buildings can be adequately spaced apart and set back from the roadway to maintain views of important visual resources.
- Program 2.4.1f Design of Residential units along the Tank Farm and Orcutt Road (Johnson to Tank Farm) scenic view corridor frontages shall be reviewed by the ARC for height, character and layout to provide optimum compatibility with the adjacent units across the streets and reduce visual impacts from these roadways to Righetti Hill.
- Program 2.4.1g Residences along “E” Street shall not be visible from the Orcutt/Tank Farm Road intersection viewing north toward Johnson Avenue.
- Program 2.4.1h Street design in the R-1 subdivision at the west base of Righetti Hill shall generally conform to Figure 2.9 to preserve optimum views from the “D” Street area toward Righetti Hill.

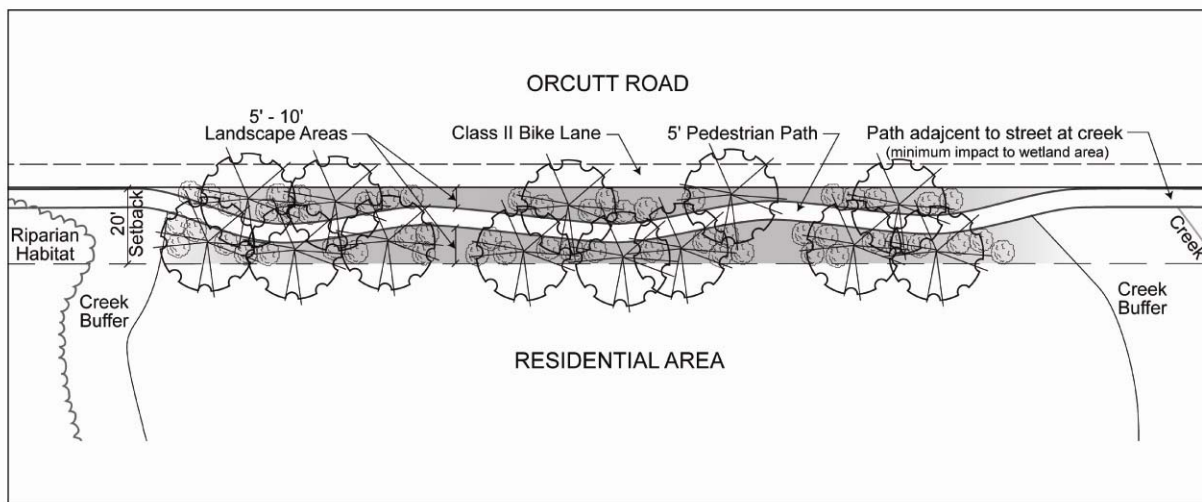


FIGURE 2.8 SEPARATED PEDESTRIAN PATH WITH LANDSCAPE BUFFER

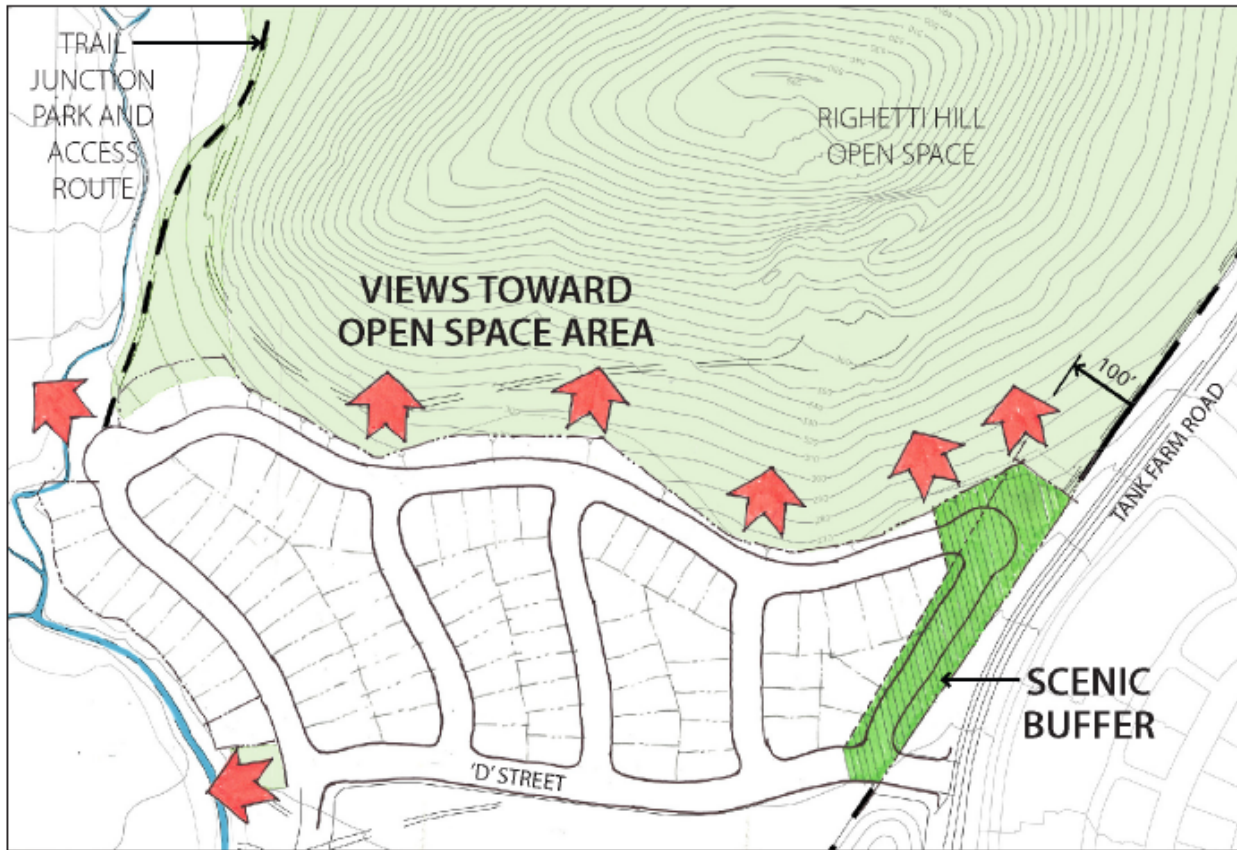


FIGURE 2.9 DIAGRAM OF STREET LAYOUT TO PRESERVE VIEWS OF RIGHETTI HILL

2.5 ARCHAEOLOGICAL AND HISTORIC RESOURCES

In the early part of the 20th century the Jacob Orcutt's 500 acre Laurel Ranch land was divided into smaller components. 114 acres covering much of the present Righetti Ranch was purchased by John Jacobson in 1906. This land was transferred to his son, Nis Jacobson, upon his death. In 1939 this property was sold to the Righetti family, parents of one of the present owners. Originally there was a house and some out buildings located in the area of the pepper trees (south side of Righetti Hill near the present day Tank Farm Road). These structures burned down, apparently some time in the 1920s or early 1930s. After the fire, a new house, barn and out-buildings were built by the Jacobson's at the present location located lower down the hillside near the present day intersection of Orcutt and Tank Farm Roads. These are the buildings that existed at the time of the Righetti purchase. An additional 30 acres was purchased from the Perozzi family in 1949 to complete the present Righetti Ranch of 144 acres. The history of ownership demonstrates that the existing farm structures at the intersection of Tank Farm and Orcutt Roads have no relationship to the historic Orcutt or Jacobson families.

Conejo Archeological Consultants' conducted a cultural records search and a limited archaeological survey in the plan area. The team identified one prehistoric site (Orcutt-1) and two isolates (Isolate 1 and

ORCUTT AREA SPECIFIC PLAN

2 Conservation, Open Space and Recreation

2) and two isolated prehistoric finds in the Orcutt Plan Area. Given the presence of an archaeological site, isolated artifacts, and historic ranch operations on the site, there is potential for buried archaeological deposits to occur within the project site.

Goal 2.5: Long-term protection of cultural resources.

Policy 2.5.1: Provide for the protection of both known and potential archaeological resources.

Program 2.5.1a Implement mitigation measures included in **Appendix C** as appropriate when entitlements in the Orcutt Plan Area are requested from the City.

Policy 2.5.2: Section 4.30 of the City of San Luis Obispo's Archeological Resource Preservation Guidelines, Mitigation Methods, and Avoidance, are hereby included by reference.

3 LAND USE AND DEVELOPMENT STANDARDS

INTENT

The City's General Plan Land Use Map designates the Orcutt Area as a major residential expansion area in the City's Sphere of Influence and anticipates annexation of this area into the City. This section contains the land use goals, policies and standards applicable to the Orcutt Area, and describes the overall development program.

The Orcutt Area is projected to provide approximately 1,000 units of housing in a wide variety of housing types, along with protection and preservation of open space features such as Righetti Hill and the extensive creek corridor within the area. Provisions are made to establish a community commercial district at the center of the neighborhood where two of the residential collector streets adjoin the neighborhood park. Land is also earmarked for a potential school site, which would include joint use of park and school facilities for recreation. The land use plan also identifies the location of the spine system of essential streets and stormwater drainage and detention basins, which support development in the plan area.

3.1 DENSITY

Appendix A, Table A-2, includes an estimate of residential development potential for each owner's property within the Orcutt Area. The table includes ranges that are based on an estimate of "net site area" for each property, which excludes creek corridors and land dedicated for street right-of-way. Achieving development within the range identified in Table A-2 is important for the overall feasibility of development in the specific plan area, because the fee program outlined in Chapter 8 relies on these unit counts to estimate the fees available to pay for area infrastructure, such as streets, bike paths and park improvements. In addition, the Orcutt Area is envisioned as an urban neighborhood within the City of San Luis Obispo, as reflected by residential development standards that allow for small lots, zero-lot line development and other land-efficient methods of residential development. The following goal and policies are intended to maintain this urban development pattern over time as land is subdivided and developed in the Orcutt Area.

Goal 3.1a: Development of the Orcutt Area as an urban neighborhood with multiple housing types, lot patterns and densities to make efficient use of land that is committed to urban development, while maintaining appropriate open space and park areas for the enjoyment of all City residents.

Policy 3.1b: *Density in the Orcutt Area will be calculated as prescribed by Section 17.16.010 of the Zoning Regulations except that dwellings with two or more bedrooms shall account for 1.0 density unit.*

Policy 3.1c: *Within new subdivisions, the City will calculate allowable density based on the net site area of the property prior to the subdivision. Density shall not be calculated on a lot by lot basis in order to accommodate the small lots allowed within the Orcutt Area (See Table 3.1 for minimum lot sizes).*

Policy 3.1d: *New subdivisions shall be designed to achieve at least the low range of units identified in Appendix A, Table A-2, unless the Community Development Director determines that physical constraints or the presence of biological resources make development of this number of units impractical, infeasible or undesirable.*

3.2 LAND USE DESIGNATIONS AND ZONING

3.2.1 Residential

Goal 3.2a: Develop residential areas that provide privacy for residents while facilitating neighborhood interaction through safe, pedestrian-friendly outdoor areas. Residential areas with attractive views, a blend of sunny and shady environs, adequate residential parking, pleasant parks, open space, and bicycle/pedestrian paths between residential areas.

Goal 3.2b: Multiple housing types of varying cost to attract a variety of homeowners and renters, with incomes ranging from extremely-low to above moderate.

Policy 3.2.1: The Orcutt Specific Plan includes Low-Density Residential (R-1), Medium-Density Residential (R-2), and Medium-High Density Residential (R-3), and High Density Residential (R-4). The allocation of area in each zone is shown in **Figure 9.1**.

Policy 3.2.2: Residential use and development standards contained in the City's Zoning Regulations shall apply to residential development in the Orcutt Area unless otherwise identified in this Specific Plan. Where standards may conflict, Orcutt Area Specific Plan standards supersede the Zoning Regulations.

Policy 3.2.3: Affordable housing will be included in all residential R-1, R-2, R-3, and R-4 zones, either by direct construction or with in lieu fees to be applied within the Orcutt Specific Plan Area. (Refer to Affordable Housing Section 3.3)

Policy 3.2.4: Uses allowed in the Orcutt Area residential land use designations shall be consistent with the City's Zoning Regulations.

Low-Density Residential (R-1-SP)

Low density residential development will be concentrated in the southern portion of the Specific Plan Area. The major portion of the R-1 development will consist of detached single-family homes with lot sizes ranging from 5,000 to 15,000 square feet. Some of the single-family residences will be located on the west side of Righetti Hill abutting the lower boundary of the Righetti Hill open space area. The highest proposed elevation on Righetti Hill for residential lots is 320 feet, although most lots will range from 260 to 310 feet in elevation.

Policy 3.2.5: Varied lot sizes, possible with zero-lot lines, are encouraged in the R-1 development area to gain efficiency, incorporate more affordable units, and avoid repetitious design. The minimum allowed lot size in the R-1 zone will be 4,500 square feet. Where creeks or other natural features constrain lot configurations there may be some larger R-1 lots (up to 15,000 square feet).

Policy 3.2.6: The development standards identified in **Table 3.1** shall apply to all R-1 development in the Orcutt Specific Plan Area.

Medium-Density Residential (R-2-SP)

The medium density residential areas will be interspersed throughout the Orcutt Area with the highest concentration in the northern portions. They are designed to provide comfortable housing for people who prefer compact residences close to the small community commercial (mixed-use) area, public services, and the neighborhood park.

Policy 3.2.7: Medium-density residential development should provide a similar sense of neighborhood cohesion as the low-density, but in a more compact arrangement.

- Policy 3.2.8: The majority of medium-density residential development should be comprised of detached and attached single-family homes; use of zero lot lines is encouraged.*
- Policy 3.2.9: Duplex housing is encouraged in the R-2 zone and should have both private and common outdoor areas. Duplex units could either be available to renters or owned as condominiums or co-housing.*
- Policy 3.2.10: The development standards identified in **Table 3.1** shall apply to all R-2 development in the Orcutt Specific Plan Area.*

Medium-High Density Residential (R-3-SP) and High Density Residential (R-4-SP)

The multi-family residential development (R-3 and R-4) will generally be concentrated in the northern and western portions of the Orcutt Area. This will facilitate easy access to the Marigold Center and commercial areas along Broad Street via Bullock Lane and Orcutt Road and will provide an appropriate transition between the urbanized city and rural county.

- Policy 3.2.11: The majority of medium-high development (R-3) should be comprised of multiplexes and multi-family apartments or condominiums.*
- Policy 3.2.12: High density development (R-4) should be developed either as multi-family apartments with common compact outdoor areas or as residential care or special needs facilities.*
- Policy 3.2.13: Multiplex dwelling units may consist of attached units with a maximum of eight units per building. Units either could be available to renters or owned as condominiums.*
- Policy 3.2.14: Multiplex and multi-family apartment units shall have access to common greens or interior parkways. Ground floor units shall be designed with private, compact outdoor areas.*
- Policy 3.2.15: All common outdoor areas within multiplexes, manufactured housing parks and multi-family apartment projects shall be privately maintained by a homeowners' association or other method acceptable to the Community Development Department..*
- Policy 3.2.16: Multifamily developments shall provide connections to pedestrian/bicycle paths in the Specific Plan Area for access to the mixed-use area, the neighborhood park, and linear park.*
- Policy 3.2.17: Manufactured housing is allowed in all zone, subject to density requirements outlined in Chapter 3.1. It is strongly encouraged as an expansion of the existing mobile home park in the northwest portion of the Plan Area.*
- Policy 3.2.18: Sites within the medium-high and high density residential zones will be made available to the Housing Authority or private developers to develop units of affordable housing for low income households. Adequate areas of medium-high density (R-3) and medium density (R-2) will be provided for additional units of affordable housing for low and moderate-income households (see Affordable Housing below for further discussion).*

Table 3.1 Residential Development Standards

Standards (minimums)	R-1	R-2	R-3	R-4
Lot area	4500 sf minimum	3000 sf minimum 5000 sf maximum Average: 4500 sf	Per City Zoning Code	Per City Zoning Code
	6000 sf average			
	with a maximum of 15,000 sf			
Lot width	45 ft minimum	35 ft minimum		
Lot Frontage on Cul De Sac	30 ft	30 ft		
Corner Lot Width	60 ft	50 ft		
Lot Depth	Varies	40 ft		
Lot Coverage	40 %	50%		
Front setback				
House	15 ft	15 ft		
Garage, carport	20 ft Must be setback at least 5 ft from the front of the house	20 ft Must be setback at least 5 ft from the front of the house		
Front Porch or Covered Entry	Required	Required		
Rear Setback				
House	20 ft	20 ft		
Garage, carport	0-5 ft	0-5 ft		
Side Setback				
First Story	0-5 ft	0-5 ft		
Second Story	0-5 ft	0-5 ft		
Street (corner lot)	10 ft	10 ft		
Garage, carport	0-5 ft	NA		

3.2.2 Community Commercial Mixed Use (CCMU - SP)

The Community Commercial zone is intended to provide an appropriately wide range of retail sales, personal service establishments, and selective office uses within the context of a distinctive, pedestrian-oriented shopping area. It is expected that these uses will primarily serve customers and clients within the Orcutt Area, but also to people community-wide without generating traffic to a level that requires arterial road access.

The southeastern portion of the City of San Luis Obispo is presently served by the Marigold Center at the intersection of Broad Street and Tank Farm Roads which contains approximately 180,000 square feet of retail floor area including a grocery store, drug store, craft store, bank and approximately 6 restaurants/food service establishments as well as seven smaller stores. This center is located about one mile away as one would drive. In addition, there is the Cross Roads Shopping center at the intersection of Broad and Orcutt (also about one mile distant) which contains a series of shops, small offices, and restaurants. The older and smaller neighborhood commercial area on Laurel Lane includes a neighborhood market and related shops but has suffered; plans for its redevelopment are being

considered. Within this larger commercial context, it is the desire of the City of San Luis Obispo to provide an appropriate community commercial area at the center of the Orcutt Area Specific Plan adjacent to the park to serve the 1,000 new units being proposed.

A viable commercial area in San Luis Obispo requires two major components in addition to adequate access: a realistic determination of the commercial mix and sustainable floor areas and a quality design setting. The Specific Plan seeks to establish the general parameters for both these components. The use and permitting aspects are considered here and the design issues are addressed in Chapter 4. While the mathematics of spending for retail uses indicate that the approximately 2,200 residents in the Specific Plan area would support around 65,400 square feet of various types of retail in the San Luis Obispo urban area, obviously deductions must be made for shopping in downtown and the other nearby centers identified in the paragraph above. An additional consideration is that while there will be support for supermarkets and food outlets, these stores come in large increments to be competitive; for example the grocery store at Marigold Center includes 60,000 square feet of floor area and is so competitive and nearby, that a supermarket would not survive in the Specific Plan area.

Within this context, this plan identifies potentially suitable types of tenants in the community commercial area. **Table 3.2** includes a typical range of categories and their suitability in the Orcutt Area. As indicated, many commercial uses are either inappropriate, do not meet access standards (related to an arterial street) or are too small to meet the threshold of size criteria.

There is the potential that at the time of development, market conditions or tenant availability may not support full development of the retail and office components of the community commercial area as identified below. In this event, at the time of project review the developer may present convincing information, including economic factors, to the City reviewing body which may approve an Administrative Use Permit to allow residential development on the ground floor. The design intent of the pedestrian-friendly street and plaza system at the intersection of “A” and “B” streets shall apply in any case.

Table 3.2 Community Commercial Uses

Type (Standard Categories)	Size (Square Feet)	Comments
Restaurants and Take-Out Foods	2,500	Small Deli and coffee shop ¹
Supermarket and Food Store	1,500	(small)attach to Deli above ²
Convenience Shops (one or two)	4,000	Hair and nail salon, flower shop, etc.
Community Center retail uses	0	Too small to make the threshold
Strip Retail and Misc.	0	Not appropriate or convenient
Furniture, Bldg. Materials, Decorator	0	Too small ³
Automotive, boats, vehicle sales & parts	0	Too small ³
Gas Stations	0	Prohibited ³
Tourist/ Specialty Centers	0	Too remote ³
Total Retail Commercial	8,000	
Financial	0	Not good location (ATM perhaps)
Local and community offices	8,500	Professional services
Total Office	8,500	

Notes:

1. Would have to be neighborhood based since unlike the Cross Roads, there is no nearby arterial.
2. Probably a small convenience food sales area attached to another operation such as a Deli. Would be to provide snacks to park and potential school users as well as neighborhood residents who do not want to drive out of the area for a full shopping expedition.
3. These uses are not appropriate and could not survive in small center which does not have arterial access. They are more land consumptive, typically require parking lots in front of the retail areas and usually are not pedestrian intense and friendly.

Goal 3.2c: A community commercial area with commercial office and retail services focusing on those living, working, or using the park in the Orcutt Area.

*Policy 3.2.19: Encourage community commercial retail uses up to a maximum of 10,000 square feet to be located on the ground floor as shown on the design plan **Figure 3.1**. Appropriate uses include small shops, grocery/deli stores, small gym/fitness facility, restaurants, and personal care services. The intent is to encourage local users who could walk to the area to participate in a small scale, pedestrian intense commercial area.*

Large scale uses generating significant traffic from other areas of the community or providing non-pedestrian related ground floor uses are not appropriate. Precluded uses are: gas stations, vehicle sales and repair, bars and taverns, night clubs, theaters, churches, building and landscaping outdoor sales, general retail of more than 3,000 sf, telecommunications facilities and banks.

Program 3.2.19a Provide small public plaza/seating areas adjacent to the intersection of “A” and “B” Streets as the focal point of the community commercial area.

Program 3.2.19b Commercial uses provided shall occupy the ground floor of the primary area as identified in **Figure 3.1**.

Program 3.2.19c Provide incentives to encourage the development of commercial uses as follows:

- Three story structures are allowed in the primary commercial area in any combination of retail on the ground floor, and either offices or residential on the upper floors.
- The off-street parking requirements for the primary commercial area may be reduced to 60% of that ordinarily required by the City zoning ordinance for the individually designated uses. Such parking may utilize group parking lots to limit the number of driveways on “A” and “B” streets.
- Commercial floor area is exempt from Orcutt Area add-on fees, but is subject to City-wide impacts, including water, sewer and transportation fees, as further discussed in Chapter 8.

Policy 3.2.20: Encourage professional and other offices uses conducive to local residents walking to work, as second floor occupancy above the retail uses identified in Policy 3.2.1.

Policy 3.2.21: Encourage home/offices (live/work) in the multifamily residential portion of the community commercial. Additional precluded uses in the community commercial would be: manufactured housing, or low intensity uses that do not contribute to a pedestrian intense environment.

3.2.3 Conservation/Open Space (C/OS-SP)

The Conservation/Open Space designation will apply to Righetti Hill generally from the 320-foot elevation to the top of the hill as a permanent zone in the Area. Chapter 2, Conservation, Open Space and Recreation provides the goals, policies, and programs for the designated open space areas.

Policy 3.2.22: Property development standards in the City's zoning regulations shall apply to Conservation/Open Space designations in the Orcutt Plan Area.

Policy 3.2.23: Uses permitted in the Orcutt Area C/OS zones shall be consistent with the City's Zoning Regulations with the exception of the following uses which are prohibited: agricultural activities, including grazing and animal keeping.

3.2.4 Agriculture

Agricultural use on the project site has included hay harvesting and cattle grazing for several decades. There are currently cattle grazing on Righetti Hill. According to the United States Department of Agriculture Soils Survey for San Luis Obispo, the plan area does not include prime soils or soils suitable for most agricultural activities. Historic usage patterns and recent experience has shown that on-site water availability for agricultural uses in the Orcutt Area is limited. The plan area does not include properties zoned Agriculture (AG); however, properties zoned agriculture and used for animal keeping and grazing activities are southeast of the plan area, across Orcutt Road in the jurisdiction of the County. In addition, several properties are currently legally used for agricultural activities under County jurisdiction. Therefore, after annexation of the Orcutt Area there will be a period of transition where existing, legal agricultural uses may be continued.

Policy 3.2.24: Minimize exposure of homeowners to agricultural nuisances.

Program 3.2.24a: The transfer of real property in the Specific Plan area will comply with the County's Right to Farm Ordinance (No. 2050). (Refer to AG-1 in **Appendix C** of this Plan.)

Program 3.2.24b: Agriculture activities in the Plan Area, including grazing and animal keeping, will be phased out and will cease by buildout. Grazing and animal keeping are not authorized uses for any land use or zoning designation in the Orcutt Plan Area, however, agricultural uses that were established legally in the County may be continued, consistent with the Chapter 17.10 (Non-Conforming Uses) of the City's Zoning Regulations.

3.2.5 Public Facilities/Special Function Uses (PF-SP)

The Public Facility zone in the Specific Plan Area is intended to provide for public recreation and education uses on public property.

Policy 3.2.5: Authorized uses in the PF-SP zone include park/playgrounds, public elementary schools, and active public recreation facilities, such as public soccer and baseball fields or tennis courts.

Policy 3.2.6: Development in the PF-SP zone is subject to the City's PF Property Development Standards included in the City's Zoning Regulations (Section 17.36.020).

Policy 3.2.7: Schools shall be allowed within the Specific Plan Area with a conditional use permit in addition to applicable state approvals or permits.

3.2.6 Elementary School

The San Luis Coastal Unified School District (SLCUSD) is the primary provider of educational services for the City of San Luis Obispo as well as other smaller communities along the coast such as Morro Bay and Los Osos. As a K-12 unified district, SLCUSD operates ten elementary schools (including two magnet schools), two middle schools, and three high schools. The Orcutt Area Specific Plan is designed to accommodate a new elementary school site. The school's location was initially planned adjacent to the neighborhood park, however, proximity to the railroad tracks and aircraft over-flight reduces the feasibility of this location. To facilitate the decision-making process regarding the school site location, SLCUSD prepared a study of four locations with the Orcutt Area, and one location just outside the

specific plan's boundaries. The study was prepared by Oasis Associates and completed on April 28, 2008, and is on file in the Community Development Department.

SLCUSD has indicated that a new school would not be needed until significant portions of the Orcutt Area and Margarita Area are developed. However, early planning for the site is needed to facilitate its development and ensure that appropriate infrastructure is in place to serve the facility. The five sites evaluated by SLCUSD are shown in Figure 3.1 and include:

Site A & E: Two different locations are identified adjacent to the Neighborhood Park

Site B: 3811 Orcutt Road (Garay)

Site C: Righetti Ranch House Site (outside of Urban Reserve Line)

Site D: Wixom Ranch (outside of OASP and Urban Reserve Line)

The top-ranked site evaluated by SLCUSD is Site B. The site is designated Low-Density Residential (R-1) and schools are a conditionally allowed use in this zone. Site C is considered in the study to be the next best option in terms of locating a school, but is located on land designated Conservation/Open Space outside of the City's Urban Reserve Line. SLCUSD is a superior agency to the City of San Luis Obispo and is encouraged, but not required, to go through the City's entitlement process prior to establishing a school site. The City does not support Site C. In general, the City's preference is to locate the school site within the street network that will be established by future OASP development. This would facilitate walking and biking to school by children living in the Orcutt Area and would be consistent with the original concept of locating the school near the neighborhood park.

3.3 AFFORDABLE HOUSING

The City's General Plan Land Use Element requires that specific plans for major residential expansion areas include sites suitable for affordable and low-income rental and owner-occupied housing. Such sites shall be integrated within neighborhoods of market rate housing and shall be architecturally compatible with the neighborhood. The specific plans will designate sufficient areas at appropriate densities to accommodate a range of dwelling types, including detached and attached single-family dwellings, "sweat-equity" housing, duplexes, apartments and condominiums, manufactured housing parks, group housing, graduated care facilities, and creative housing cooperatives. To meet this requirement, the City will solicit and support new housing developments that include one or more of the following features:

1. Subdivisions designed to integrate various housing types, densities, and costs.
2. Affordable "Starter" housing consisting of small (approximately 1,150-1,450 square feet) homes on lots of 5,000 square feet or less.
3. Duplexes and "garden homes" which provide the desirability and appearance of single-family housing while allowing higher density and lower housing costs.
4. Self-help housing development of approximately 20-30 small, single-story detached homes.
5. Medium Density and Medium-High Density apartments, condominiums or manufactured housing.
6. Special needs housing designed for seniors, handicapped persons, farm workers, large families, graduated care facilities, or others with special physical needs and low incomes.

Developers may choose to build one or more housing types, and to work with housing non-profits such as Peoples' Self-Help Housing Corporation, the San Luis Obispo Housing Authority, Habitat for Humanity, or other agencies or individuals to cooperatively plan, develop, and market affordable housing within their developments.

City of San Luis Obispo Inclusionary Housing Policy

San Luis Obispo has adopted an inclusionary housing program that requires all new development projects to include affordable housing units, dedicate real property for affordable housing, or pay an in-lieu fee to increase affordable housing opportunities Citywide. In residential annexation areas like the Orcutt Area, at least 5 percent of the new housing must be rented or sold at prices affordable to low income households. Another 10 percent of the new housing must be available for moderate income households.

New housing in San Luis Obispo must address the community's urgent need for affordable housing. For housing to qualify as "affordable," the housing developer must guarantee that the housing units will be developed and maintained in a manner consistent with the City's Affordable Housing Standards, which are updated annually with maximum sales prices and income limits for potential purchasers of affordable homes.

As laid out in the following policies and programs, all of the required affordable housing will be constructed within the Orcutt Area. The affordable housing requirement will be met either by dedicating land in new subdivisions to the San Luis Obispo Housing Authority, or other City recognized low-income housing developer, or by building affordable units as part of the project. Property owners may also work together to coordinate development of the required number of affordable units (5% low and 10% moderate). When land is dedicated in-lieu of providing the affordable housing units, all frontage improvements and off-site improvements required to serve the affordable housing development shall be installed by the market-rate housing developer.

Orcutt Area Affordable Housing

Goal 3.3: Multiple housing types of varying cost that attract a variety of homeowners and renters, with incomes ranging from very-low to high.

Policy 3.3.1: The City's inclusionary housing requirements shall be met by building the affordable units within the Orcutt Specific Plan Area.

Policy 3.3.2: Each development project within the Orcutt Area shall construct a minimum 10 percent of moderate income affordable dwelling units (ADU) and 5% low income ADU's at the time of development, or dedicate land for affordable housing consistent with Policy 3.3.4.

Policy 3.3.3: To promote reasonable efficiency a project developer may coordinate with another Orcutt Area property owner or developer to provide the required affordable dwelling units when the units proposed are less than 10.

Policy 3.3.4: Land dedication in-lieu of building affordable housing: Developers of residential subdivisions may dedicate land to the Housing Authority, or other City recognized low-income housing developer, in-lieu of constructing the required affordable housing units. Land that is dedicated for the purpose of developing affordable housing must be of sufficient size to construct at least the number of low and/or moderate income units required by the Inclusionary Housing Ordinance for the project, plus 25% to accommodate the allowed density bonus. When land is provided to meet the affordable housing requirement, all frontage improvements and required off-site improvements shall be installed by the market-rate housing developer. In general, land dedicated for affordable housing shall be dispersed throughout subdivisions, instead of clustered. The requirement to disperse affordable housing shall not be construed to prevent dedication of land suitable for an affordable apartment project. The decision to accept land dedication in-lieu of building affordable housing in a subdivision is under the discretion of the City Council at the time of Tentative Subdivision Map review.

These affordable housing units are exempt from growth management requirements.

Program 3.3.4a: Provisions of State law allow a minimum 25-percent density bonus for providing affordable housing above and beyond the required percentages (5 percent lower-income and 10 percent moderate-income) when State-mandated standards are met.

The City's Affordable Housing Incentives (SLOMC 17.90) provide additional incentives for affordable housing. The City will support the development of affordable housing in the Orcutt Area through State of California and City incentives.

Policy 3.3.5: Community Commercial Mixed Use development projects are exempt from inclusionary housing requirements.

Policy 3.3.6: Encourage an extension of the existing manufactured housing park, south of the existing Willow Creek Mobile Home Park in the R-3 zone which could accommodate approximately 75 units of low-income and moderate-income housing.

Policy 3.3.7: Encourage Creative Living Environments. In residential expansion areas, City policies call for specific plans to incorporate opportunities for individuals or small groups, other than the specific plan developer, to build homes or create personalized living environments suited to individuals, families, and small groups or to accommodate those with special needs. Cooperatives or co-housing developments and community land trusts are ways residents can create personal living space supported by group dining, meeting, recreation facilities, and services. The City encourages land dedication to promote this housing option.

3.4 PUBLIC SAFETY

The residents of San Luis Obispo and the Orcutt Area may be subject to natural and human-caused hazards during their lifetime. Natural processes such as earthquakes, landslides, flooding, and wildfire can become hazards when they disrupt or otherwise affect the lives and property of people. Human-caused hazards can include the improper use and/or disposal of hazardous material, use, and/or construction of unsafe structures.

Goal 3.4: Protect life and property from natural and environmental hazards.

Policy 3.4.1: Avoid hazards associated with ground shaking, liquefaction, and slope instability.

Program 3.4.1a: Geotechnical Study. A geotechnical study shall be prepared by a State registered engineering geologist for each project site prior to site development. This report shall include an analysis of the liquefaction potential of the underlying materials according to the most current liquefaction analysis procedures, an evaluation of the potential for soil settlement beneath the project site, and an evaluation of the potential for expansive soils. If the site is confirmed to be in an area prone to seismically-induced liquefaction, have a high potential for settlement or have expansive soils, appropriate techniques to minimize liquefaction potential, reduce settlement impacts or stabilize slopes shall be prescribed and implemented.

Program 3.4.1b: All structures, transportation infrastructure, and subgrades shall comply with applicable methods of the Uniform Building Code and all transportation

infrastructure shall comply with the most current California Department of Transportation design standards.

Policy 3.4.2: Avoid exposure to hazardous environmental conditions.

Program 3.4.2a: Prior to development in areas not surveyed for the Limited Phase 1 Environmental Site Assessment (Rincon Consultants, Inc., 2005) a Phase 1 Environmental Site Assessment shall be conducted to identify the presence of recognized environmental conditions associated with soil and groundwater contamination at the site. If recognized conditions are encountered then a Phase II Environmental Site Assessment shall be performed to determine if soil or groundwater have been affected.

Program 3.4.2b: The following areas or situations shall be subject to environmental assessment prior to public access and/or development:

- a. Farmhouses or buildings associated with prior agricultural uses. Prior to demolition, the buildings shall be assessed for release and/or storage of hazardous materials. If it is determined that there may have been hazardous materials release, a Phase II Environmental Site Assessment shall be performed to determine if soil or groundwater has been affected.
- b. 55- Gallon drums in the Plan Area. Prior to removal, soils samples shall be taken in the vicinity of the drums and analyzed for total extractable petroleum hydrocarbons (TEPH) by EPA method 8015, heavy metals by CCR Title 22 metals, and solvents by EPA method 8260B. If levels of contaminants are found to exist in concentrations that exceed regulatory thresholds, the extent of contamination shall be delineated and an appropriate remediation method implemented.

3.5 AIRPORT COMPATIBILITY

The Orcutt Area is located in the vicinity of the San Luis Obispo County Regional Airport, and it is subject to the requirements of the San Luis Obispo County Regional Airport Land Use Plan (ALUP). The OASP has been determined by the Airport Land Use Commission to be consistent with the ALUP, because it is designed to permit only the types of uses and development intensities that are allowed in Safety Area S-2 of the ALUP. In addition, the City and Airport Land Use Commission have carefully reviewed land uses in the Orcutt Area as part of detailing a Detailed Area Plan. In particular, the City worked with the ALUC to determine that a school site, previously located immediately west of the Neighborhood Park, was not compatible with the ALUP and it was removed from the Plan. The following goal and policies are intended to provide for on-going consistency between the OASP and ALUP. In addition, Table A-3 (Appendix A) calculates the maximum allowable density on each property in the Orcutt Area, including potential density bonuses, and shows that this development is consistent with the applicable safety policies for ALUP Safety Area S-2.

Goal 3.5. Develop Orcutt Area consistent with the County's Airport Land Use Plan

Policy 3.5.1: Minimize the risk of potential aircraft accidents in the vicinity of the Airport by avoiding the development of land uses and land use conditions which pose hazards to aircraft in flight.

ORCUTT AREA SPECIFIC PLAN
3 Land Use and Development Standards

- Policy 3.5.2: Ensure that no structure, landscaping, apparatus, or other feature, whether temporary or permanent in nature shall constitute an obstruction to air navigation or a hazard to air navigation.*
- Policy 3.5.3: This specific plan shall serve as the Detailed Area Plan as required by the Airport Land Use Plan.*
- Policy 3.5.4: Although no land in the Orcutt Area is suitable to be used as Airport Compatible Open Space because of existing topography, transmission lines and other constraints, the City will continue to pursue its open space and greenbelt acquisition policies, which serve to create large areas of Airport Compatible Open Space in the areas bordering the Urban Reserve Line and the SLO County Regional Airport.*

Types and Intensities of Land Use

The types and intensities of land uses described earlier in this chapter are defined consistent with the County Airport Land Use Plan. The residential densities and their locations concentrate residential development in locations where it will minimize issues with Airport compatibility. Table A-3, Appendix A, shows the maximum number of units allowed on each property in the Orcutt Area, consistent with Airport Land Use Plan standards.

Performance Standards for Airport Compatibility

- 3.5.2a Risk of Injury: No use shall be established and no activity conducted which entails a risk of physical injury to operators or passengers of aircraft.
- 3.5.2b Airspace Protection: No object or structure may be erected, and no plant allowed to mature, to penetrate any “imaginary surface” as defined in Federal Aviation Regulations Part 77. Any proposed feature approaching these surfaces will be referred to the airport manager for review and recommendation. Building within the height limits of this specific plan will not approach the FAA imaginary surfaces.
- 3.5.2c Operations Interference: No use shall be established which produces visually significant quantities of smoke.
- 3.5.2d Bird Attractants: No use shall be established and no activity conducted which attracts birds to the extent of creating a significant hazard of bird strikes. Examples are outdoor storage or disposal of food or grain, or large, artificial water features. This provision is not intended to prevent enhancement or protection of existing wetlands, the mitigation of impacts to wetlands or construction of required detention basins.
- 3.5.2e Indoor Noise: Indoor space for the following uses shall provide a maximum noise exposure not exceeding 45 decibels (dB) Community Noise Equivalent Level, and a 60 dB maximum for aircraft single events: dwellings; offices and incidental meeting rooms; public reception areas; worker break rooms; research, development, and production areas where concentration is required and interior-generated noise is typically at or below the level of an office.

- 3.5.2f Lighting: Exterior lighting that is difficult to distinguish from typical airport lighting and glare in the eyes of pilots using the airport is prohibited.
- 3.5.2g Avigation Easements: At the time of subdivision development, avigation easements shall be recorded for each affected parcel in a form approved by the County of San Luis Obispo Airport Land Use Commission.
- 3.5.2h Real Estate Disclosure: All owners, potential purchasers, occupants (whether as owners or renters), and potential occupants (whether as owners or renters) shall receive full and accurate disclosure concerning the noise, safety, or overflight impacts associated with airport operations prior to entering any contractual obligation to purchase, lease, rent, or otherwise occupy any property or properties within the airport area. The San Luis Obispo Airport Land Use Commission shall approve the format of the disclosure.

FIGURE 3.1 POTENTIAL ORCUTT AREA SCHOOL SITES





FIGURE 3.2 COMMUNITY COMMERCIAL MIXED USE DESIGN CONCEPT

FIGURE 3.3 – COMMUNITY COMMERCIAL MIXED-USE CONCEPT SKETCHES



View looking Southeast from “A” Street toward Righetti Hill, across the Neighborhood Park.



View Looking Northwest at the Community Commercial center from the Neighborhood Park.

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4 COMMUNITY DESIGN

4.1 DESIGN QUALITY AND CHARACTER

San Luis Obispo’s downtown and residential areas reflect varied architectural styles and a creative design character, which gives our city a unique architectural flavor among the central coast communities. However, within some of the individual residential areas, there are examples of either too much homogeneity of design or incompatible combinations of architectural extremes. In an effort to promote a compatible but diverse character for the Orcutt Area, an emphasis has been placed upon the Craftsman, California Bungalow and California Mission Revival styles. The architectural characteristics of these styles highlight the historic aspects of the area while allowing flexible design, which can be adapted to the rural nature of the site.

Creative design should incorporate elements that harmonize with, and take advantage of the Mediterranean climate of the Central Coast, including the indoor-outdoor relationship of the residence to the adjacent landscape, as well as principles of sustainable design and energy efficiency, including “green building.” Cookie-cutter type repetition should be avoided through individual variation and alternation of unit design that respect the views and shape of the lots. It is expected that individual subdivisions and planned developments will be used to implement the general densities identified within the Specific Plan to allow greater flexibility in lot layout and unit design such as zero lot-line units, garages in the rear of units and harmonious massing of units along residential streets.

While this Chapter incorporates many standards and guidelines, it is emphasized that guidelines are designed to focus attention on the special features desired in the implementation of the Orcutt Area Specific Plan. Given the character of the potential development in the Specific Plan, almost all projects will be reviewed by the City’s Architectural Review Commission (ARC). As such this plan seeks to avoid replication of the information, standards, guidelines and processing information contained in the ARC Guidelines.

Goal 4.1: New development in the Orcutt Area that is well designed, internally compatible and enhances San Luis Obispo’s unique sense of place.

Policy 4.1.1: Encourage a compatible mix of residence designs.

Program 4.1.1a: The architectural styles of Craftsman, California Bungalow, and California Mission architectural themes are strongly encouraged in the Orcutt Area as illustrated in **Figures 4.1.**

Program 4.1.1b Design Standards for R-1 and R-2 districts. Zoning design and building development standards are identified in **Table 3.1** and include some modifications to City standards that are unique to the Orcutt Area Specific Plan for the R-1 and R-2 zoning districts. These design standards (DS) shall apply to all R-2 development; R-1 development is encouraged to observe them as well.

DS-1 Refer to **Table 3.1** for R-1 and R-2 development standards

DS-2 All residences are required to have entries that front the street unless a parking court configuration is utilized.

DS-3 All residential lots adjacent to creek/riparian corridors must use open fencing, if any.

DS-4 Inter neighborhood connectivity: Project site designs shall incorporate road, pedestrian and bicycle connections into the adjacent neighborhoods

(especially important as there are so many individual land owners) and provide future connection points to development in future phases.

Program 4.1.1c Design Guidelines for R-1 and R-2 districts. These design guidelines (DG) supplement the mandatory elements and standards identified in other chapters of this document. In addition these design guidelines also supplement the City's standard ARC Guidelines to clearly define for owners, builders, architects and designers the desired character of the Orcutt Area residential neighborhood.

Section 1: Site Planning

DG 1.1 Encourage pedestrian connections to "A" street retail: Projects adjacent to "A" and "B" streets should provide supplemental pedestrian and bike access to these streets to facilitate non-automobile access to retail and office uses located there.

DG 1.2 Pedestrian and bicycle connections are encouraged to connect to the creek trail system and the identified bicycle route system as identified on Figure 2.4. These routes should be designed to encourage short cuts to desirable locations to make walking and biking more convenient.

DG 1.3 Internal street layout should provide loop circulation in preference to dead end cul-de-sacs.

DG 1.4 Streets and paths should incorporate views of local vistas or landmarks and reasonably direct connect to amenity features such as parks, creek path systems and community areas.

DG 1.5 Neckdown curbs (bulbouts) at intersections and decorative paving at crosswalks at primary intersections, entries and at parks or recreation areas are encouraged. See Figure 4.2a and 5.8.

DG 1.6 Public alleys: Alleys are encouraged where developments face major streets or where alternative parking solutions to conventional street driveways are desired. They also can allow homes to face parks, creek areas or vistas by relaxing automobile access to the front of the residential unit. If utilized, alleys should follow design principles below:

- Alley should be straight from one end to the other to facilitate visibility and safety.
- Dead-end alleys should be less than 300' long.
- Landscaping should be consistent with the rest of the development with a 4' minimum parkway strip and one street tree per lot.
- Each lot should provide light from a fixture mounted on either a structure or a pavement pedestal.

Section 2: Lot Site Design and Building Configuration

DG 2.1 Driveways: Shared driveways/curb cuts are encouraged with zero lot line garages/houses and landscape planters. (Figure 4.2 plan and Figure 4.4-b California Bungalow example)

DG 2.2 Garage Location: A major factor affecting the character of the streetscape is the location of the garage. To minimize the potential negative visual impact of garages and parking aprons, the following actions are identified subject to individual project review by City:

- Alternatives to the standard 18' driveway curb cut are encouraged. The desired goal is to limit standard 18' driveways to 60% or less in any project greater than eight units in the R-1 or R-2 zones.

- Other alternatives include single lane driveways to a garage at the rear of the residential unit, shared driveways for two units, recessed garages which allow a necked down/landscaped apron and alley access. These options are shown in Figure 4.1-b: Typical garage locations and proportions.
 - Where standard driveways are included, there should be no more than two in an adjacent or consecutive lot configuration.
- DG 2.3 Side-Drive or Alley Accessed Garages: Side-drive designs with rear yard garages and parking aprons provide design variety and reduced visual emphasis on garages from the street are a preferred alternative to the standard garage location. Examples of this approach are shown in Figure 4.2-b.
- DG 2.4 Parking Courts: This design approach has the advantages of increasing density and reducing the impact of curb cuts. However there is the potential disadvantage of minimizing the visibility of residential entries and maximizing views of garage doors as seen from the street. To reduce these potential impacts the following actions are identified (see Figure 4.3):
- Garages should be recessed behind the homes' main façade similar to the guidelines for standard lot homes increasing the visibility of the entry and reducing the impact of garage doors and apron parking.
 - Parking courts should have accent paving which emphasizes the pedestrian route to the entry as well as breaking up the visual expanse of concrete or asphalt paved area.
 - Landscape areas, including trees, are encouraged to break up paving expanse and views of garages.
- DG 2.5 Position of Structure: Where feasible, the longest portion of any structure is encouraged to face within 32 degrees of south for improved solar access.

Section 3: Building Design; Elements, Colors and Materials

- DG 3.1 Design Themes: Architectural variations within general residential themes shall be encouraged through the use of Craftsman, California Bungalow or California Mission styles. (Figure 4.4)
- DG 3.2 Landscape Character: A unifying residential landscape character is encouraged through open or low fencing and utilization of native plant species.
- DG 3.3 Driveway Materials: Alternative paving materials are recommended for driveways and other residential paths such as stamped/colored concrete, paving stones, tiles, bricks, or City approved permeable paving materials. Use of mid-driveway landscape strips is encouraged.
- DG 3.4 Porches: All residences are encouraged to have covered porches. These porches may project up to 7 feet from the front property line with design review by the City. (Figures 4.4 and 4.5-a)
- DG 3.5 Front Yards: Residences are encouraged to provide small patios or lawn play areas with consistent landscaping. Patios with low fence walls, open fences or hedges, and trellis coverings are preferred. (Figure 4.5-a)
- DG 3.6 Mix of Residential Heights: In predominantly 2-story projects which exceed 20 units, 20% of the homes should be single story scattered throughout the project. In order to reduce the building mass facing the street, the second story portion should generally be located in the rear of

the unit (defined as being set back 20 from the front main façade of the unit), or to one side or in two story bays. (Figure 4.5-b)

- DG 3.7 Façade Elements: It is desirable to create multiple elements in the façade of two story structures to reduce the visual mass. The residential design should break the structure into three to four distinct elements such as entry, main structure, single story element and the roof. Two story vertical gabled bays and roof dormers can also add variety. (Figure 4.5-b and 4.5-c)
- DG 3.8 Roof Overhangs: A variety of hips and gables should be used, particularly on the front/street façade to further break up the mass of the structure. Roofs extended over windows for shading and associated brackets are encouraged.
- DG 3.9 Surface Materials: In developments of more than four homes a minimum of two material pallets are encouraged each with a different primary material. (A primary material is the material used on a minimum of 67% of the building façade; e.g. stucco, wood.) An alternative is to have some of the homes utilize two materials wherein the second material must cover at least 40% of the visible façade.
- DG 3.10 Roofing Materials: In developments of more than four homes a minimum use of two primary roof materials such as concrete shake, Spanish tile or composition shingles is encouraged.
- DG 3.11 Color Palettes: In developments of over four homes, it is strongly encouraged to have a minimum of two colors from different color families for each primary body material, such as stucco and/or wood. A minimum of two trim colors shall be selected for each primary color (but not necessarily used on each house). Within an individual building, color variety should relate to a change in materials (stucco to wood) or body material to trim material.
- DG 3.12 A solar energy source such as solar panels or solar roofing is encouraged per conservation/open space policies.

Program 4.1.1d: The following design standards shall apply to all R-3 and R-4 development in the Orcutt Specific Plan Area:

- DS-5 R-3 and R-4 standards set forth in the City Zoning Ordinance shall apply in addition to the standards and guidelines provided in this section.
- DS-6 Parking Rear Setback: 0-5 ft
- DS-7 All ground floor units will have covered porches/entries in the front (door facing common areas) of the unit.
- DS-8 All units adjacent to creek/riparian corridors must use open fencing, if any.

Program 4.1.1e: Residential R-3 and R-4 development is encouraged to observe the following guidelines in addition to the adopted ARC Community Design Guidelines:

- DG 4.1 Position of Structure: Where feasible, the longest portion of any structure is encouraged to face within 32 degrees of south for improved solar access.
- DG 4.2 Paving Materials: Alternative paving is recommended for driveways and other residential paths such as stamped/colored concrete, paving stones, tiles, and bricks.
- DG 4.3 Scale: Projects over eight units should be broken up into multiple structures. Facades over 150 feet in length should be avoided.

- DG 4.4 Façade Elements: Creation of multiple elements in the façade of two and three story structures to reduce the visual mass is strongly encouraged. The architectural design should break the structure into three to four distinct elements such as entry, main structure, single story element and the roof. Vertical gabled bays and roof dormers can also add variety. (See also Figures 4.5-a and 4.5-b for examples of multiple façade elements.)
- DG 4.5 Roof Overhangs: A variety of hips and gables should be used, particularly on the front/street façade to further break up the mass of the structure. Roofs extended over windows for shading and associated brackets are encouraged. Secondary hipped or gabled roofs covering the entire mass of a building are preferable to mansard roofs or segments of pitched roof at the edge of the structure.
- DG 4.6 Surface and Roofing Materials: In developments of more than four units a minimum of two material pallets are encouraged. See DG 3.8 and DG 3.9 for detailed language.
- DG 4.7 Color Palettes: In developments of over six units, it is strongly encouraged to have a minimum of two colors from different color families for each primary body material, such as stucco and/or wood. A minimum of two trim colors shall be selected for each primary color (but not necessarily used on each unit). Within an individual building, color variety should relate to a change in materials (stucco to wood) or body material to trim material.
- DG 4.8 Solar Panels: A solar energy source such as solar panels or solar roofing is recommended per conservation/open space policies.
- DG 4.9 Manufactured Housing Foundations: Foundations shall be enclosed or skirted.

Policy 4.1.2: Foster neighborhood connectivity

Program 4.1.2a: Residential development design should use local streets **configured to** enhance neighborhood atmosphere and limit through traffic. Where cul-de-sacs back up to parks or open space, pedestrian/bicycle paths shall be provided to connect the cul-de-sac to the park or open space area.

Program 4.1.2b: Design features such as front porches, front yards along streets and entryways facing public walkways, should also be incorporated into residential design to strengthen neighborhood atmosphere.

Program 4.1.2c: Universally accessible entries are encouraged for all buildings, including single-family houses.

Policy 4.1.3: Development along the lower slopes of Righetti Hill shall respect existing elevation contours and shall be designed consistent with Section 7.2, Hillside Development, of the Community Design Guidelines

4.2 COMMUNITY COMMERCIAL DESIGN

Special design attention has been given to the intersection of “A” and “B” streets where there is provision for mixed residential and community commercial zone adjacent to the neighborhood park. Three story structures with notable architectural features are encouraged at this intersection to provide the design focal element in the Orcutt Area community. However, the design of these buildings should not replicate downtown, rather they should key off designs and themes used in the surrounding residential neighborhood, and building design and materials should reflect the site context.

Goal 4.2a: A dense pedestrian friendly residential and mixed-use commercial neighborhood that terminates at the public park for the urban frontage on “A” Street from the Power Line easement to “B” Street/Park.

Policy 4.2.3 Develop an urban focal point at the intersection of “A” and “B” street.

Program 4.2.3a Provide a traffic intersection with traffic calming design, extensive pedestrian sidewalks with textured surfaces and extensive curbside landscaping.

*Program 4.2.3b Define the northern side of the space with two public plazas that may be used for gathering and eating. The adjacent structures shall be a minimum of two stories tall, may be three in height, and shaped to shelter the plazas and contrast with the view toward the open space of the park, the banks of trees along the confluence of the creeks and Righetti Hill and Islay hill in the background. See example described in **Figure 3.1** and **Figure 3.2b**.*

Program 4.2.3c Define the southern portion of the intersection by landscaping (to contrast with the urban character of Program 4.2.1b above) with low-lying fences, perhaps small playgrounds and a view of the large open soccer field as a base to the vistas toward Righetti and Islay Hills which are major defining features of this neighborhood.

Policy 4.2.4 Develop the character of “A” Street for this sector to provide for a pedestrian friendly environment incorporating compact urban forms, wide sidewalks, street trees, limited driveway access and zero or reduced building frontage setbacks.

Program 4.2.4a Provide building setbacks from “A” street as follows:

From “B” Street to the first residential street to the north, zero setback: (the intent is to provide a gateway vista toward the natural areas identified above.)

From the first residential street to the power line easement, 10 feet setback: (expected to be residential structures which will require defined entries, but the facades shall still define the street as an open room.)

Program 4.2.4b Provide trees in tree wells for the whole sector with the intent of developing a canopy over the sidewalks and a major portion of “A” Street.

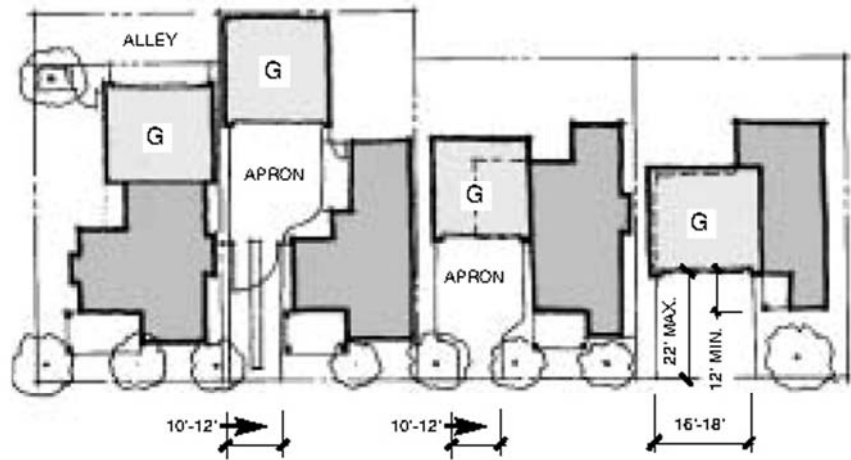
Program 4.2.4c In order to achieve a cohesive neighborhood character, building facades, materials, colors, entries and windows should be consistent and in harmony with previously constructed neighboring properties.

Goal 4.2b: An architectural focal point for the Orcutt Area neighborhood located at the intersection of “A” and “B” Streets.

Policy 4.2.1 The mixed use residential and community commercial structures at the intersection of “A” and “B” street are encouraged to be three stories in height, provide special public plazas and have a unique architectural character that will serve as a local landmark.

- Program 4.2.1a **Figures 3.1** and **3.2** are examples of the planning that is encouraged for this intersection and shall be utilized by the Architectural Review Commission when designs are submitted to insure that the general intent of Goal 4.2 is met.
- Program 4.2.1b Strict application of the height ordinance shall be relaxed to enable suitable architectural features to be added to the structure at the intersection of “A” and “B” Streets.
- Policy 4.2.2 The designated residential units fronting “A” Street for the mixed-use zone designated in the Specific Plan shall be designed to provide a strong pedestrian corridor leading to the focal point designated in Policy 4.1. The location of all structures, their street setbacks, and landscaping as well as architectural features shall contribute to a strong, harmonious sense of place which progresses toward the community center.*

FIGURE 4.1 R-1 SITE DESIGN CONCEPTS



Alley Access Attached or Detached	Side-Drive, Rear -Yard, Attached or Detached	Semi-Recessed Front Yard Apron	Garage with Standard Front Yard Apron.
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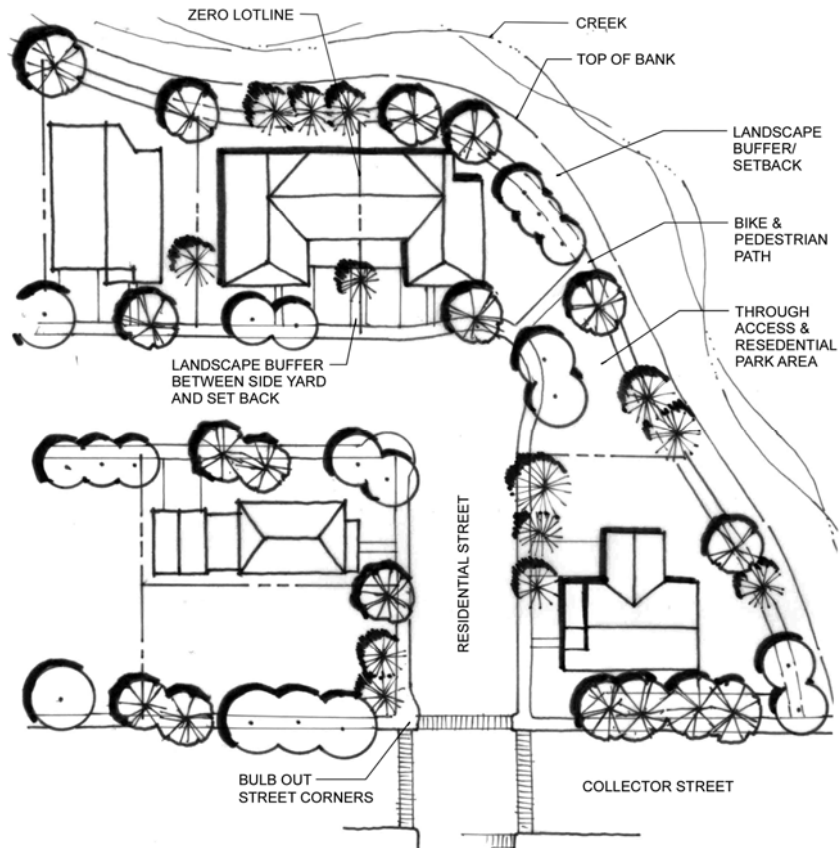


FIGURE 4.2 R-2 SITE DESIGN CONCEPTS

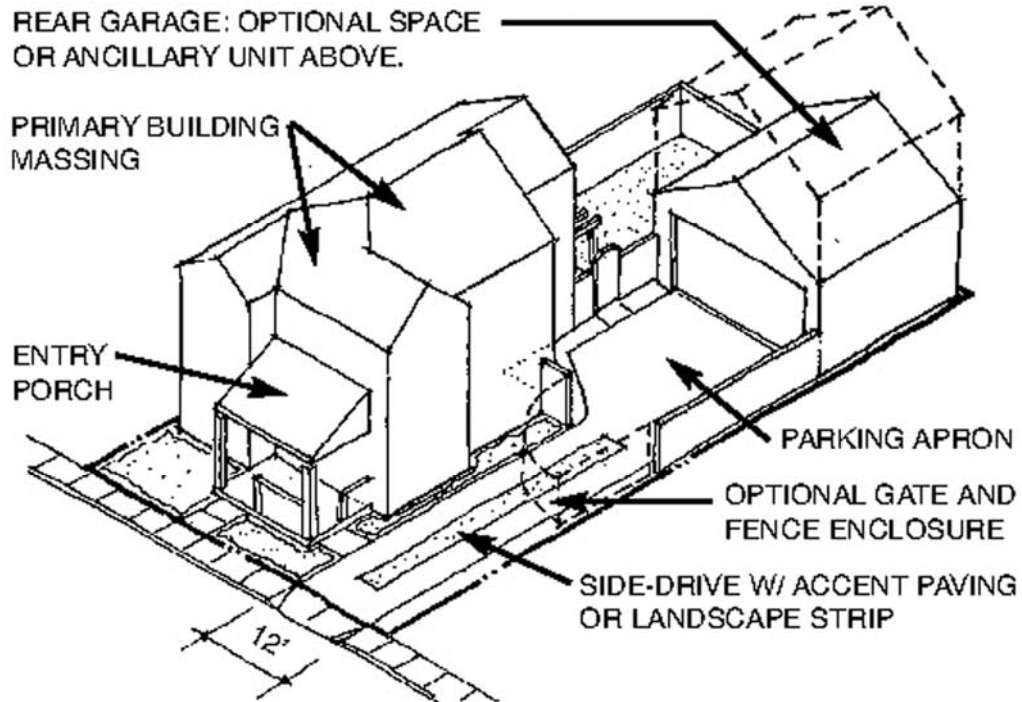
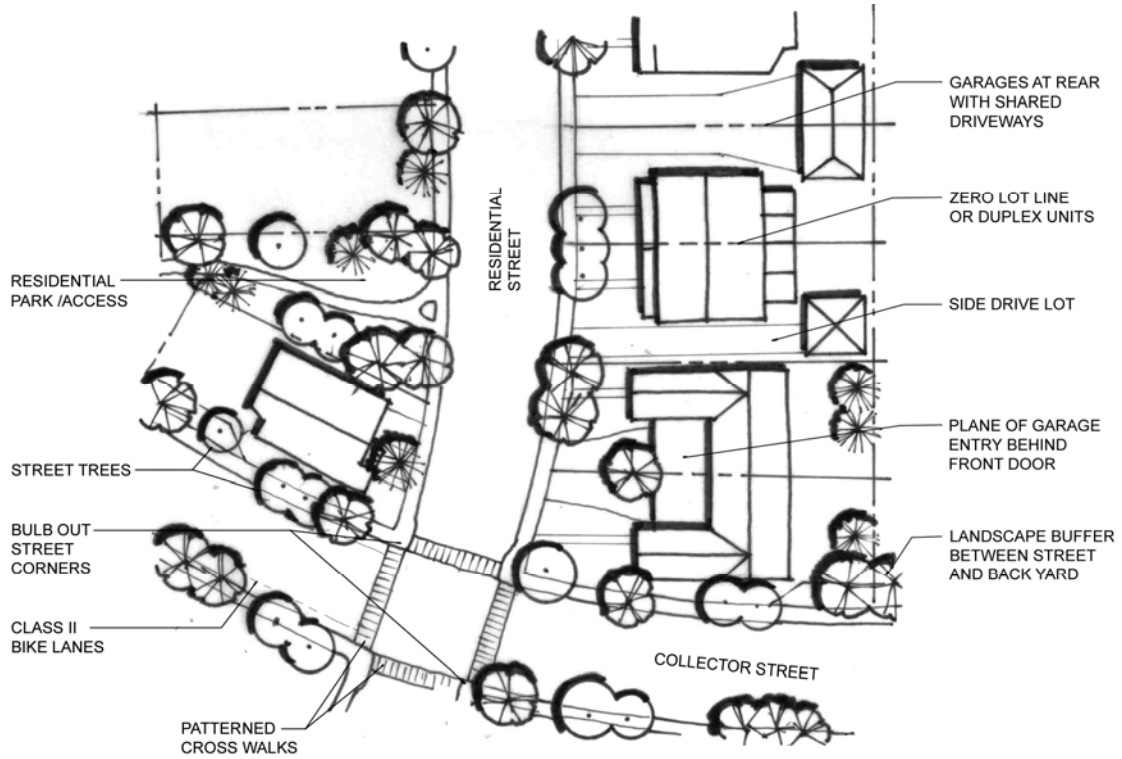
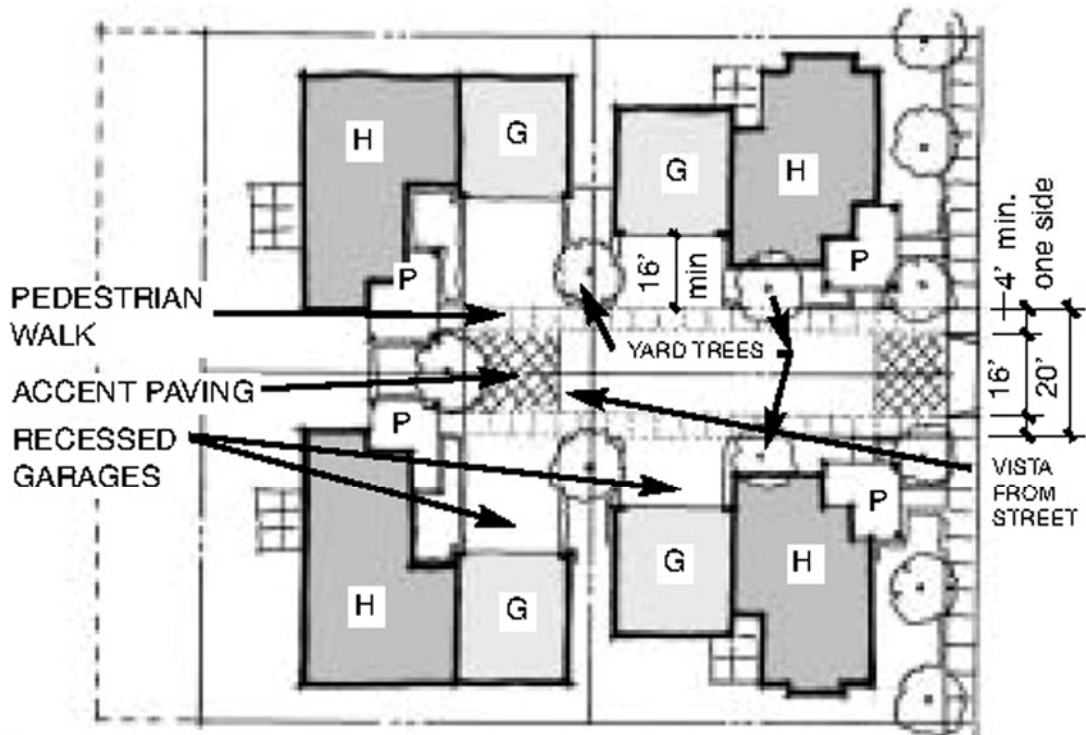
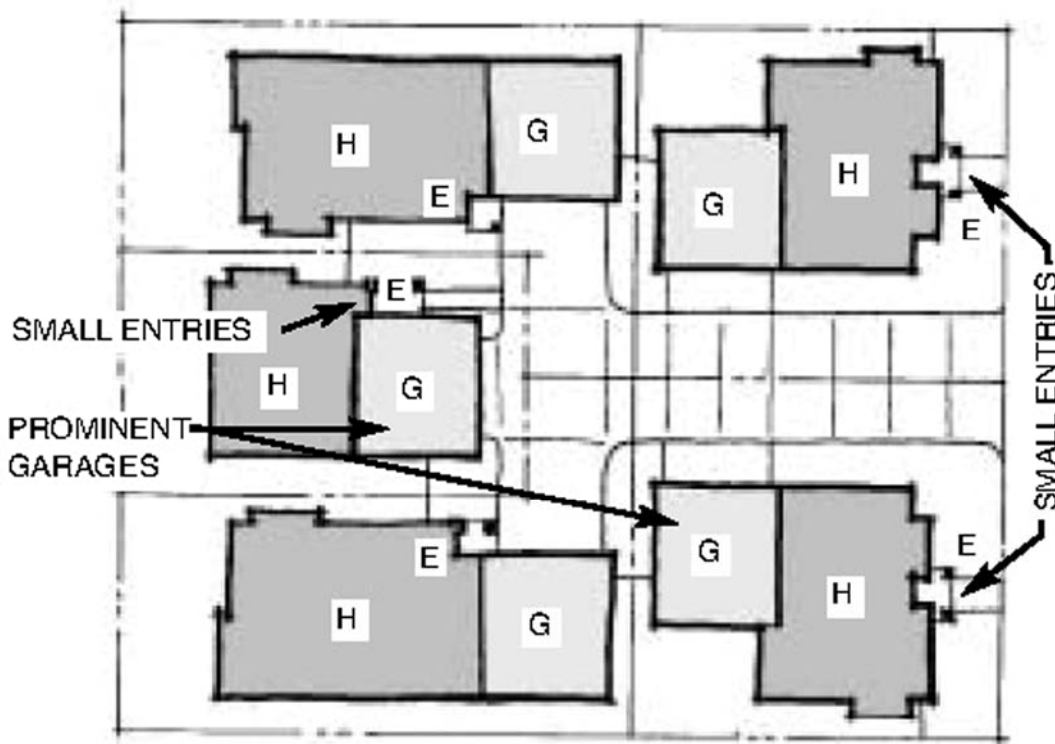


FIGURE 4.3 PARKING COURTS



DESIRABLE PARKING COURT



UNDESIRABLE PARKING COURT

FIGURE 4.4 ARCHITECTURAL DESIGN CONCEPTS (R-1 AND R-2 ZONES)



CALIFORNIA MISSION

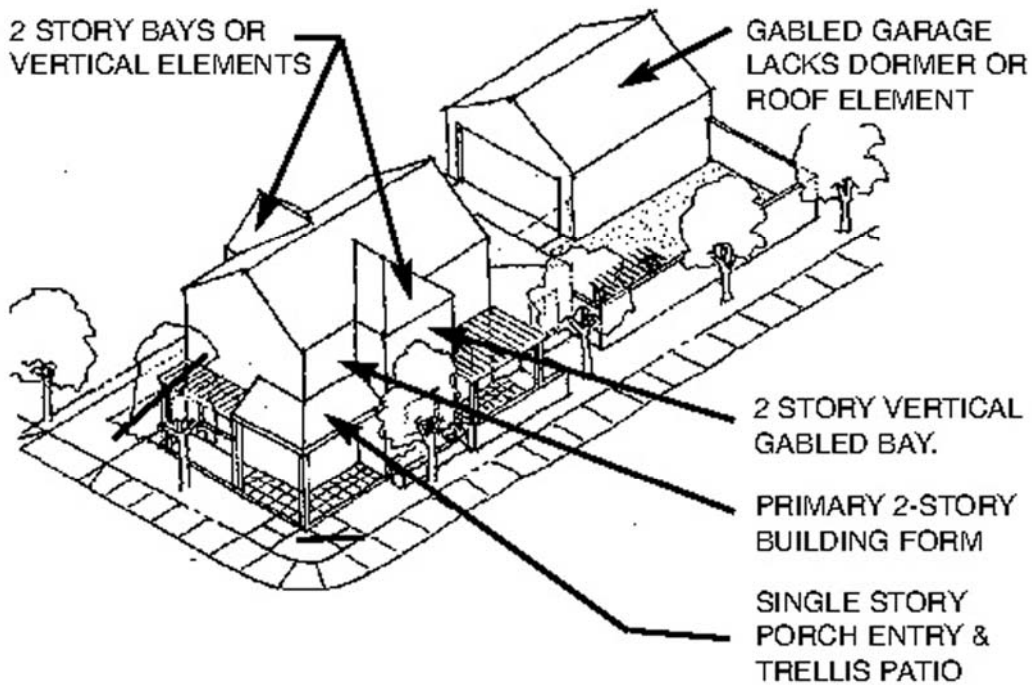


CALIFORNIA BUNGALOW

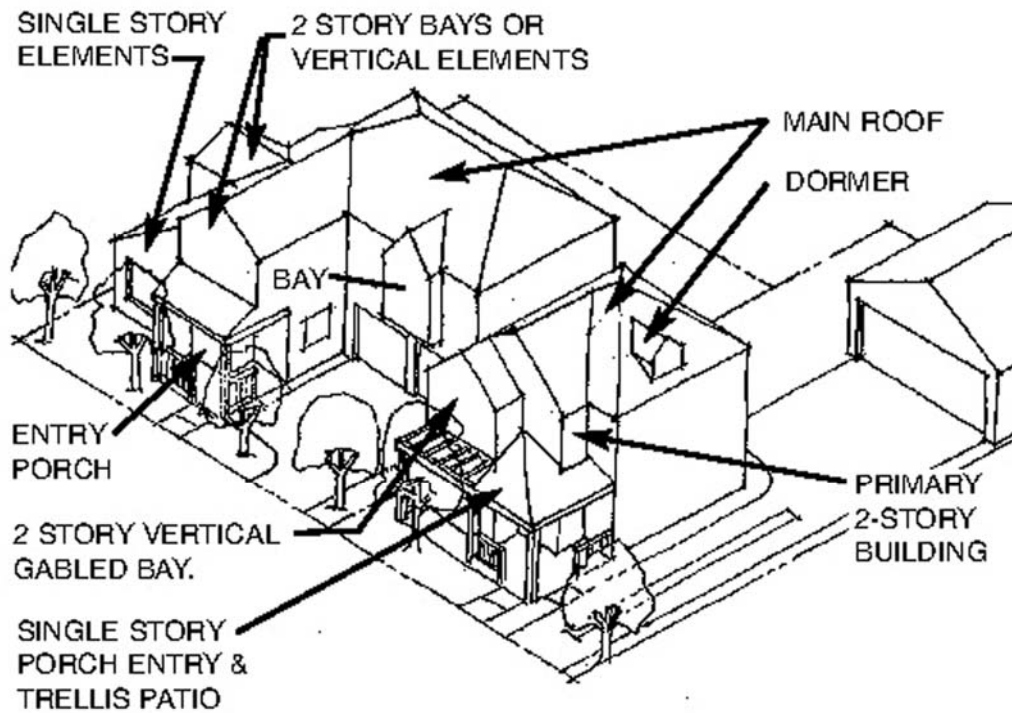


CRAFTSMAN

FIGURE 4.5 R-2 RESIDENTIAL DESIGN CONCEPTS

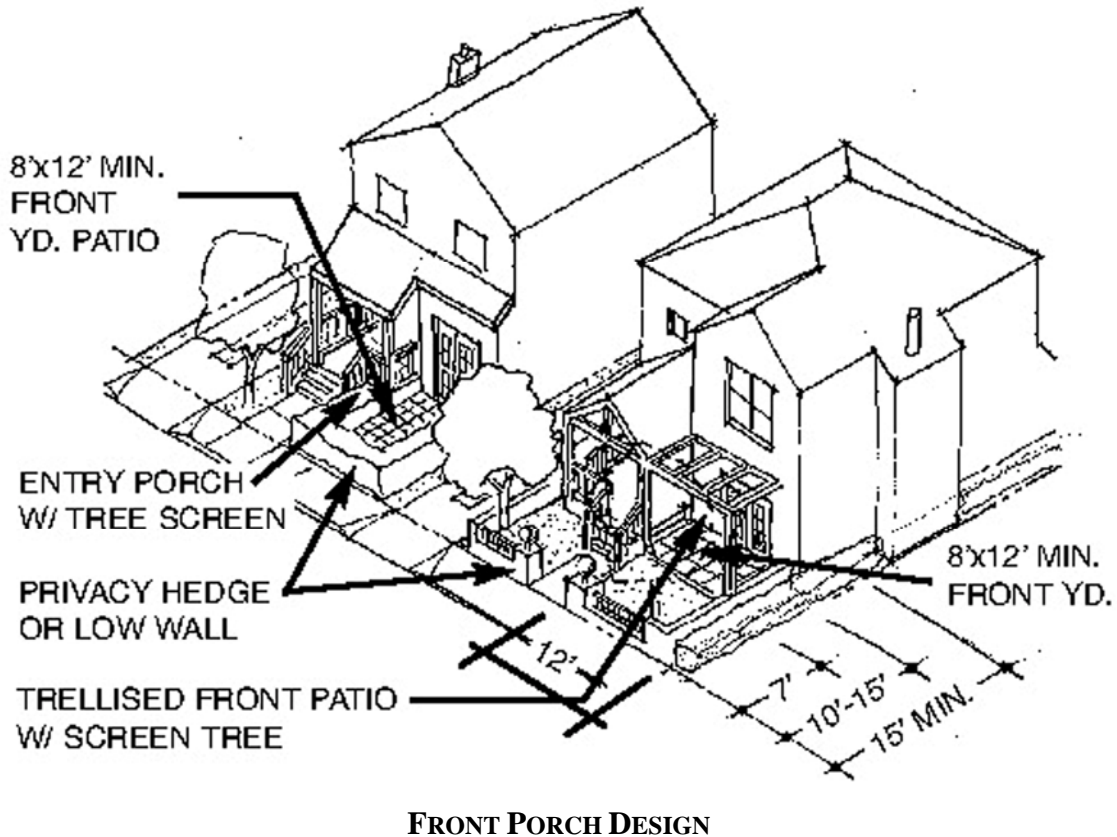


VERTICAL DESIGN ELEMENTS INCORPORATED



VARIED BUILDING MASSING

FIGURE 4.5 (CONTINUED) : R-2 RESIDENTIAL DESIGN CONCEPTS



FRONT PORCH DESIGN

4.3 LANDSCAPING

Goal 4.3: An attractive and sustainable landscape pattern that unifies and enhances the quality of the development in the Orcutt Area

Policy 4.3.1: The landscape characteristics should complement the architectural styles described previously. Trees, shrubs, and groundcovers should be chosen and sited to emphasize their natural shape, form, and structure.

Policy 4.3.2: Residential and public area landscapes are encouraged to incorporate California native plant species and other non-native plant species with lower water requirements and drought hardiness. Plants that require low amounts of watering are preferable for the area to reduce the citywide demand for water. **Appendix D** includes a plant palette with drought resistant and wind resistant species that are recommended for the Orcutt Specific Plan Area.

Policy 4.3.3: Development landscaping provided within the Specific Plan Area shall abide by the requirements of the City Arborist for construction. Requirements for tree protection include but are not limited to: construction setbacks from trees; safety fencing around trees; grading limits around the base of trees as required; and a replacement plan for trees removed including replacement at a minimum 1:1 ratio.

Policy 4.3.4: Native riparian areas are sensitive to invasive plant species competing with the native species. Non-native, invasive species shall not be transferred into the riparian

corridor. Turf shall be situated and maintained to avoid spreading into riparian/creek corridor, or avoided completely.

Program 4.3.4a: To ensure that project landscaping does not introduce invasive non-native plant and tree species to the Specific Plan area, project proponents shall submit a final landscaping plan for review and approval of the City Arborist and Community Development Director. The California Invasive Plant Council (Cal-IPC) maintains several lists of the most important invasive plants to avoid. Project proponents are encouraged to refer to the lists when creating a plant palette for landscaping to ensure that plants on the lists are not used.

Landscaping plans shall include the following information, at a minimum:

- The species, diameter at breast height, location, and condition of all existing trees;
- Identification of trees that will be retained, removed, or relocated;
- Location and size of plant and tree species proposed to be planted;
- The location of proposed utilities, driveways, street tree locations, and the size and species of proposed street trees; and
- A reclaimed water irrigation plan

Program 4.3.4b: The following plant species shall not be planted within the Specific Plan area:

- Arundo donax* (Giant reed)
- Bamboo grass species (except in containers)
- Cortaderia species* (Pampas Grass)
- Cynodon dactylon* (Bermuda grass)
- Eucalyptus species
- Hedera canariensis* (Algerian ivy)
- Phalaris arundinacea* (Ribbon grass)
- Toxicodendron diversiloba* (Poison oak)
- Ricinus communis* (Castor bean)
- Senecio mikaniodes* (German ivy)
- Vinca major* (Bigleaf periwinkle)

Policy 4.2.5: *The City of San Luis Obispo provides a list of acceptable street trees, of which those listed in **Table 4.1** are recommended for each street to provide a sense of place or unity in the neighborhood.*

TABLE 4.1 RECOMMENDED STREET TREES

Orcutt Road (Laurel to Johnson):	'A', 'B', 'C' & 'D' Street:	Orcutt Road (Johnson to Tank Farm):
<ul style="list-style-type: none"> <input type="checkbox"/> <i>Acer rubrum</i> (Red Maple) <input type="checkbox"/> <i>Crataegus phaenopyrum</i> (Washington Hawthorn) <input type="checkbox"/> <i>Liquidambar styraciflua</i> (Sweet gum) <input type="checkbox"/> <i>Pistacia chinensis</i> (Chinese Pistache) <input type="checkbox"/> <i>Quercus agrifolia</i> (Coast live oak) 	<ul style="list-style-type: none"> <input type="checkbox"/> <i>Calodendrum capense</i> (Cape Chestnut) <input type="checkbox"/> <i>Geijera parviflora</i> (Australian Willow) <input type="checkbox"/> <i>Pistacia chinensis</i> (Chinese Pistache) <input type="checkbox"/> <i>Quercus agrifolia</i> (Coast live oak) <input type="checkbox"/> <i>Quercus suber</i> (Cork oak) <input type="checkbox"/> <i>Stenocarpus sinuatus</i> (Firewheel Tree) 	<ul style="list-style-type: none"> <input type="checkbox"/> <i>Quercus agrifolia</i> (Coast live oak) <input type="checkbox"/> <i>Quercus ilex</i> (holly oak) <input type="checkbox"/> <i>Quercus suber</i> (Cork oak)
Residential Streets:		
<ul style="list-style-type: none"> <input type="checkbox"/> <i>Acer rubrum</i> (Red Maple) <input type="checkbox"/> <i>Betula nigra</i> (River Birch) <input type="checkbox"/> <i>Calocedrus decurrens</i> (Incense Cedar) <input type="checkbox"/> <i>Calodendrum capense</i> (Cape Chestnut) <input type="checkbox"/> <i>Cedrus deodara</i> (Deodar Cedar) <input type="checkbox"/> <i>Cinnamomum camphorum</i> (Camphor Tree) <input type="checkbox"/> <i>Convallaria majalis</i> (Lily of the Valley Tree) <input type="checkbox"/> <i>Crataegus phaenopyrum</i> (Washington Hawthorn) <input type="checkbox"/> <i>Ficus retusa nitida</i> (Indian laurel fig) <input type="checkbox"/> <i>Geijera parviflora</i> (Australian Willow) <input type="checkbox"/> <i>Ginkgo Biloba</i> (Ginko) <input type="checkbox"/> <i>Gleditsia trioncanthos inermis</i> (Honey Locust) <input type="checkbox"/> <i>Jacaranda mimosifolia</i> (Jacaranda) <input type="checkbox"/> <i>Koelreuteria paniculata</i> (Goldenrain Tree) <input type="checkbox"/> <i>Liquidambar styraciflua</i> (Sweet Gum) 	<ul style="list-style-type: none"> <input type="checkbox"/> <i>Magnolia grandiflora</i> (Southern Magnolia) <input type="checkbox"/> <i>Metrosideros tomentosus</i> (New Zealand Christmas) <input type="checkbox"/> <i>Pinus canariensis</i> (Canary Island Pine) <input type="checkbox"/> <i>Pinus muricata</i> (Bishop Pine) <input type="checkbox"/> <i>Pinus pinea</i> (Italian Stone Pine) <input type="checkbox"/> <i>Pistacia chinensis</i> (Chinese Pistache) <input type="checkbox"/> <i>Platanus racemosa</i> (California Sycamore) <input type="checkbox"/> <i>Podocarpus gracilior</i> (Fern Pine Tree) <input type="checkbox"/> <i>Quercus agrifolia</i> (Coast Live Oak) <input type="checkbox"/> <i>Quercus ilex</i> (Holly Oak) <input type="checkbox"/> <i>Quercus suber</i> (Cork Oak) <input type="checkbox"/> <i>Stenocarpus sinuatus</i> (Firewheel Tree) <input type="checkbox"/> <i>Tristania conferta</i> (Brisbane box) <input type="checkbox"/> <i>Ulmus parvifolia</i> (Chinese Evergreen Elm) 	

4.4 LIGHTING

Illumination for streets, public spaces, and private grounds shall be energy-efficient, glare reducing and directed away from public areas. Fixtures such as the ones shown in **Figure 4.3** are appropriate for the planned architectural character of the Orcutt Specific Plan area.

Goal 4.4: Maintain compatible exterior lighting.

Policy 4.4.1: Exterior lighting in all portions of the Orcutt Area shall be compatible with the architectural and landscape design and not create a nuisance for adjacent and nearby neighbors.

Policy 4.4.2: All lighting within the specific plan area shall comply with the lighting standards contained in the San Luis Obispo Community Design Guidelines and the City's night-sky ordinance..

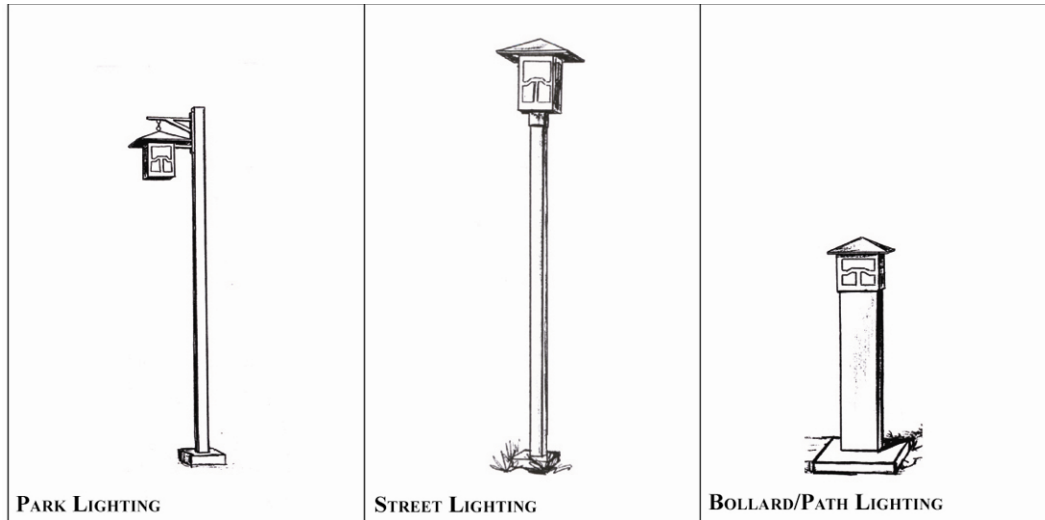
Policy 4.4.3: In addition to the overall community standards, the following standards shall apply to all outdoor illumination of streets, public spaces, and private grounds.

Program 4.4.3a: Orcutt Area Lighting Standards:

1. Lighting shall compliment the architectural style planned for the area. See **Figure 4.3** for suggested typical lighting fixture design
2. Lighting fixtures shall not be mounted more than 16 feet high; bollards will not exceed 3 feet in height.
3. Lighting fixtures shall be energy efficient.
4. All lighting fixtures should be properly shielded to eliminate light and glare from affecting adjacent and other properties and passing vehicles or pedestrians.
5. Trail lighting shall be at a scale appropriate for pedestrians, utilizing bollards, although overhead lighting may be used where vandalism of bollard lights is a concern.
6. Illumination toward the sky shall be minimized.
7. Night lighting of public areas shall be kept to the minimum necessary for safety and security purposes.
8. Exterior lighting within 100 feet of open space shall be shielded and aimed as needed to avoid spillover into open space areas. Decorative lighting shall be low intensity and meet City standards.

Policy 4.4.4: Pedestrian lighting should be used to reinforce a pedestrian scale in the Community-Commercial center, but the lighting should be different than the Downtown San Luis Obispo lighting standard.

FIGURE 4.6 SUGGESTED TYPICAL SITE LIGHTING



4.5 NOISE

Sources of anticipated noise in the Orcutt Area range from the Union Pacific Railroad and traffic along Orcutt Road to recreational activities at the neighborhood park. The ambient noise level resulting from the various noise conditions listed above are measured using the Ldn and CNEL noise descriptors using the decibel (dB) scale.

The General Plan Noise Element establishes maximum noise exposure levels for various uses. Under the General Plan, residential outdoor activity areas have a maximum allowable noise level of 60 dB and residential indoor areas have a maximum allowable noise level of 45 dB.

Existing Conditions

Existing noise sources in the Orcutt Area include vehicular traffic on roads and highways, particularly the north portion of Orcutt Road and Tank Farm Road; train operations along the Union Pacific Railroad (UPRR); and airport activities at the San Luis Obispo County Regional Airport. The Orcutt Plan Area is located adjacent to the UPRR railroad (western portion) and approximately two miles northeast of the airport. In the City's 1996 Noise Element, the City included a map of future noise contours based on projected traffic and train trips. The City updated the mapping in 1998. The 60 and 65 A-weighted decibel (dba) contours extend 352 and 163 feet, respectively, from the centerline of the railroad track and 135 and 60 feet, respectively, from the centerlines of Orcutt Road and Tank Farm Road. Currently there are an average of five freight and four passenger train trips per day, mostly occurring during the day. The Noise Element projected ten freight and four passenger trains per day. Planning for the Orcutt Area attempts to maintain enjoyable and healthy conditions in the new neighborhood with consideration that new passenger and freight trains may be added in the future.

Future Conditions

Noise levels will change as the Orcutt Area is developed. New noise sources will include temporary, construction-related activities and long-term project-generated traffic. Traffic will incrementally increase along roads within the Project Area and the project vicinity. The existing

sensitive noise receptors are the existing residences along Tank Farm and Orcutt Roads. The following policies, programs, and standards are intended to minimize impacts to existing and future sensitive receptors.

Noise Reduction Techniques

Goal 4.5: Residential development consistent with the General Plan Noise Element and City Noise Ordinance.

Union Pacific Railroad

Policy 4.5.1: New development shall reduce indoor and outdoor noise levels resulting from the UPRR by including noise mitigation techniques in design and during construction of neighborhood layout and individual residences. Noise levels for outdoor use areas shall be reduced to 60 dB or less and indoor noise levels to 45 dB in the residential neighborhoods along the railroad tracks (Figure 4.4).

Program 4.5.1a: A distance buffer, approximately 160 feet wide, should be provided between the centerline of the tracks and the western boundary of the residential areas. This buffer may be part of the linear park/floodable terrace for stormwater detention. The buffer will reduce sound levels to approximately 65 dB for outdoor areas of the nearest residences. In addition, a portion of “C” Street is located between the linear park/floodable terrace adding an additional 48-foot buffer between the tracks and the residential areas to the west and southwest of the park. The additional buffer provided by “C” Street will further reduce the outdoor noise level in these residential areas.

Program 4.5.1b: Where the buffer is not adequate, a combination low berm and sound wall will be constructed between the buffer area and the residential neighborhood. The berm/wall barrier shall be landscaped with screening vegetation and will be approximately 8-10 feet high. The low berm and sound wall combination will reduce sound levels another 5 dB for outdoor areas in the residential areas.

Program 4.5.1c: Residential uses near the railroad tracks should be oriented away from the railroad. To the greatest feasible extent, windows should be faced away from the railroad tracks. Balconies, if any are included, should also face away from the railroad tracks. Useable backyards should have fencing or orientation such that the line-of-sight to the railroad tracks is blocked.

Program 4.5.1d: In residential developments with parking lots, the parking lots should be placed between the residence and the railroad tracks to provide additional buffer.

Program 4.5.1e: Design of residences along the railroad track should locate sensitive uses in the residences away from the tracks. Within the residences, bathrooms and kitchens should be located toward the railroad, while bedrooms and living areas should be located away from the railroad.

Program 4.5.1f: Development should follow normal construction practices and City and State Building Code requirements. Use of noise reducing building materials, such as special noise attenuating windows, shall be used to further reduce indoor noise levels by insulating against outdoor noise sources. This will reduce indoor noise levels by 15 dB.

Orcutt Road and Tank Farm Road

- Policy 4.5.2: New development shall reduce indoor and outdoor noise levels resulting from the surrounding arterials by including noise mitigation techniques in design and construction of neighborhood layout and individual residences. (See footnote above) Outdoor noise levels shall be reduced to 60 dB or less and indoor noise levels to 45 dB in the residential neighborhoods along Orcutt Road and Tank Farm Road (Figure 4.5).*
- Program 4.5.2a:** Outdoor activity areas should be setback from the centerline of Orcutt Road and Tank Farm Road by a minimum of 80 feet. Outdoor activity areas will be sheltered from Orcutt Road and Tank Farm Road by the garage and residences. These measures will reduce sound levels to approximately 65 dB for outdoor areas of the residences. The 80-foot setback requirement may be waived if the applicant can show that the use of a combination of landscape and building elements meets the intent of this program by reducing outdoor sound levels to 60 dB or lower.
- Program 4.5.2b:** Design of residences along Orcutt Road and Tank Farm Road should locate sensitive uses in the residences away from the roads. Within the residences, bathrooms and kitchens should be located toward the roads, while bedrooms and living areas should be located away from the roads.
- Program 4.5.2c:** A distance buffer, approximately 60 feet wide will be provided between the centerline of Orcutt Road and Tank Farm Road and the residential portions of the dwellings along Orcutt Road and Tank Farm Road. This buffer will reduce noise levels inside residences to approximately 60 dB. Approximately 20 feet of this buffer along Orcutt Road and Tank Farm Road will be landscaped and will have a separated pedestrian path.
- Program 4.5.2d:** Development shall follow normal construction practices and City and State Building Code requirements. Use of noise reducing building materials, such as special noise attenuating windows, shall be used to reduce indoor noise levels by insulating against outdoor noise sources.

4.6 PUBLIC ART

- Goal 4.6:** **Development in the Orcutt Area that enhances the City's cultural and aesthetic environment.**
- Policy 4.6.1: Public visual arts shall be included in the Orcutt Area, in conformance with citywide policies and requirements.*
- Policy 4.6.2: The neighborhood park, the principal collector streets, the Public School and the Community Commercial areas are favored locations for public art.*
- Policy 4.6.3: Public art should be integrated into the Orcutt Area and not limited to designated locations.*

4.7 ENERGY CONSERVATION

- Policy 4.7.1: Subdivision design, site planning and building design should take advantage of all reasonable opportunities to reduce energy and other resource consumption, in compliance with the General Plan Conservation and Open Space Element's (COSE) Solar Access Policies and Standards for subdivisions and residential uses. (Refer to the COSE, Section 4.23)*

ORCUTT AREA SPECIFIC PLAN

4 Community Design

- Policy 4.7.2: New development in the Orcutt Area is encouraged to follow energy efficient project design, with emphasis on the use of daylight and solar exposure.*
- Program 4.7.2a: The placement of a building on a site and the building itself should be designed to maximize opportunities for the optimal operation of passive systems for heating, cooling, and lighting. Sunlight should be used for direct heating and illumination whenever possible. Natural ventilation and shading should be used to cool a building.
- Program 4.7.2b: The use of exterior shading devices, skylights, daylighting controls, high performance glazing that allows the transmission of light with minimal heat gain, and high thermal mass building components is encouraged.
- Program 4.7.2c: Residential developments with five or more dwelling units and non-residential projects over 5,000 square feet of gross floor area shall comply with the City's green building certification system. (Refer to COSE Policy 5.30.7)
- Program 4.7.2d: City policy requires that 5% of all single-family units constructed shall utilize Photovoltaic solar collectors which can feed excess current back into the electric grid. After 2008, the use of photovoltaics for single-family units is required to increase by 4% per year until the year 2020. (As of January 2007, solar systems are eligible for incentive funds based on system performance.)
- Policy 4.7.3: New residential and commercial construction shall achieve improved energy conservation through the use of energy efficient appliances and lighting.*
- Program 4.7.3a: All new residential units shall incorporate Energy Star compliant appliances.
- Program 4.7.3b: Where reasonably possible, new residential and commercial construction shall incorporate compact fluorescent light bulbs. Exceptions may be considered for exterior lights, accent lighting or special fixtures.

FIGURE 4.7 RAILROAD NOISE REDUCTION

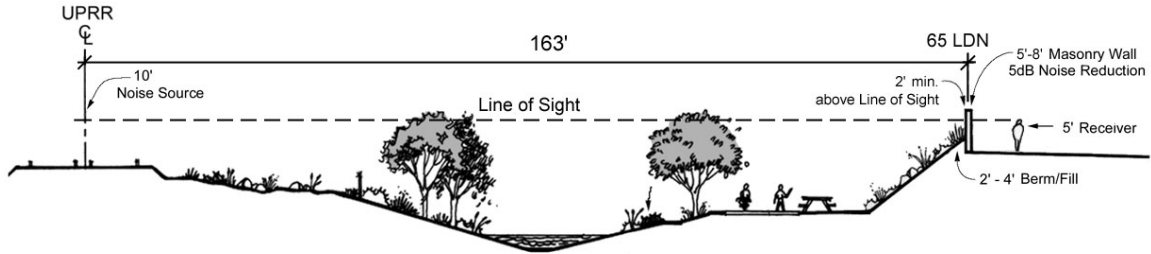
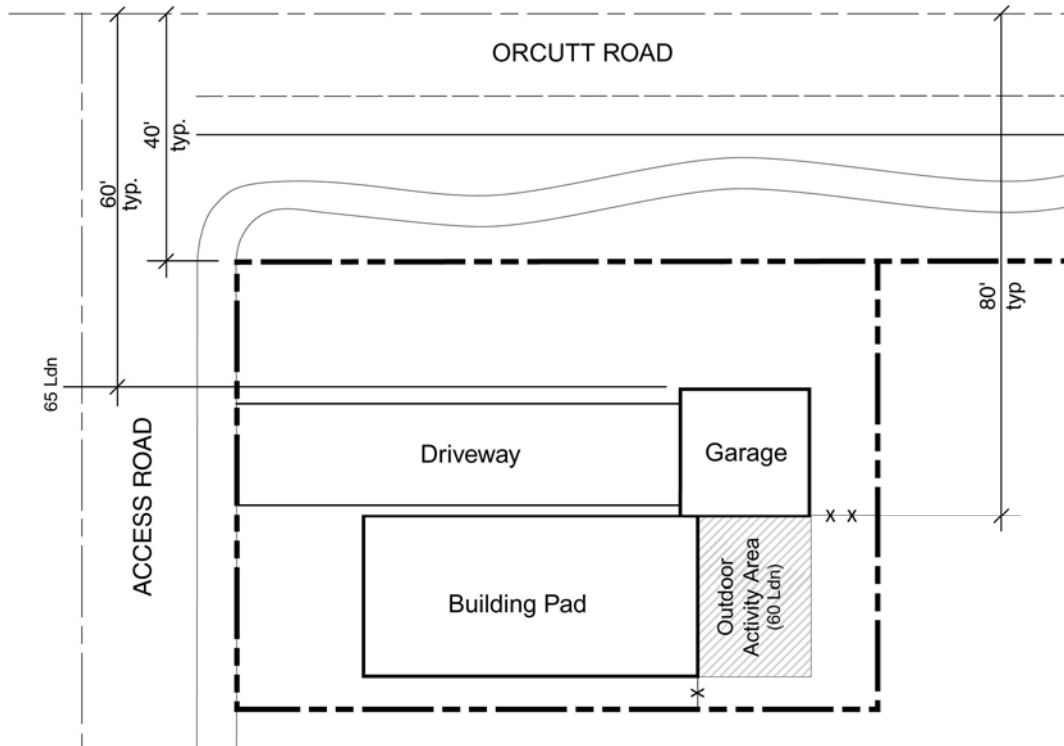


FIGURE 4.8 ORCUTT ROAD/TANK FARM ROAD NOISE SETBACKS



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5 CIRCULATION

Intent

The primary objective of the circulation plan is to provide access to, and through, the Orcutt Area by developing a network of interior roadways and linking these to existing arterials adjacent to the Orcutt Area. Development of circulation in the Orcutt Area also includes provision of circulation systems that support alternative modes of transportation.

The circulation plan provides direct connections between the existing arterials (Orcutt Road and Tank Farm Road) and the new roads in the Orcutt Area (**Figure 5.1**). These connections will facilitate access for Orcutt Area residents to downtown, the airport and Highway 1 via Broad Street and other major arterials. The Orcutt Area circulation plan incorporates specific recommendations of the City's General Plan (amended in 2006) and the City's recent area-wide traffic study for the southern portion of San Luis Obispo. While the proposed design provides connections to existing arterials, it seeks to minimize creation of new thoroughfares between Orcutt Road and Tank Farm Road or Johnson Avenue and Prado Road that would encourage substantial increases in traffic through the existing and future residential neighborhoods in the southern portion of the City. Internal circulation in the Orcutt Area is designed to promote traffic calming and moderate vehicle speeds while maintaining safe traffic flow. The traffic calming features, discussed in the following sections, will provide for the safety of pedestrians while maintaining circulation. The overall goals guiding the Orcutt Area Specific Plan's circulation are:

- Goal 5.1:** **A circulation system that is conducive to efficiency and safety which accommodates new trips generated by development within the Orcutt Area.**
- Goal 5.2:** **Direct connection to existing arterial streets and access to other parts of the City.**
- Goal 5.3:** **Bicycle and pedestrian circulation routes, and access to public transit service that promotes these modes of transportation.**

5.1 ARTERIALS

Orcutt and Tank Farm Roads are the arterial streets serving the Orcutt Area. Orcutt Road is contiguous with two sides of the Specific Plan Area and consists of two lanes with Class II bicycle lanes on both sides of the road from Johnson Avenue to Tank Farm Road. Per the Specific Plan, Orcutt Road will have connections with three new roads in the Orcutt Area: the residential collectors "A" and "B" and the local "E" Street. Traffic accessing the Orcutt Area from the north will use Orcutt Road and "A" Street or Bullock Lane. Tank Farm Road and "D" Street provide access from the south. Based on the traffic study prepared for the Program EIR, development of the Orcutt Area is expected to add 628 Average Daily Trips (ADT) to Orcutt Road between Johnson and Tank Farm at build-out. Levels of service on Orcutt Road will not change and no additional through traffic lanes on Orcutt Road between Johnson and Tank Farm Roads are anticipated, though an additional turn lane will be added.

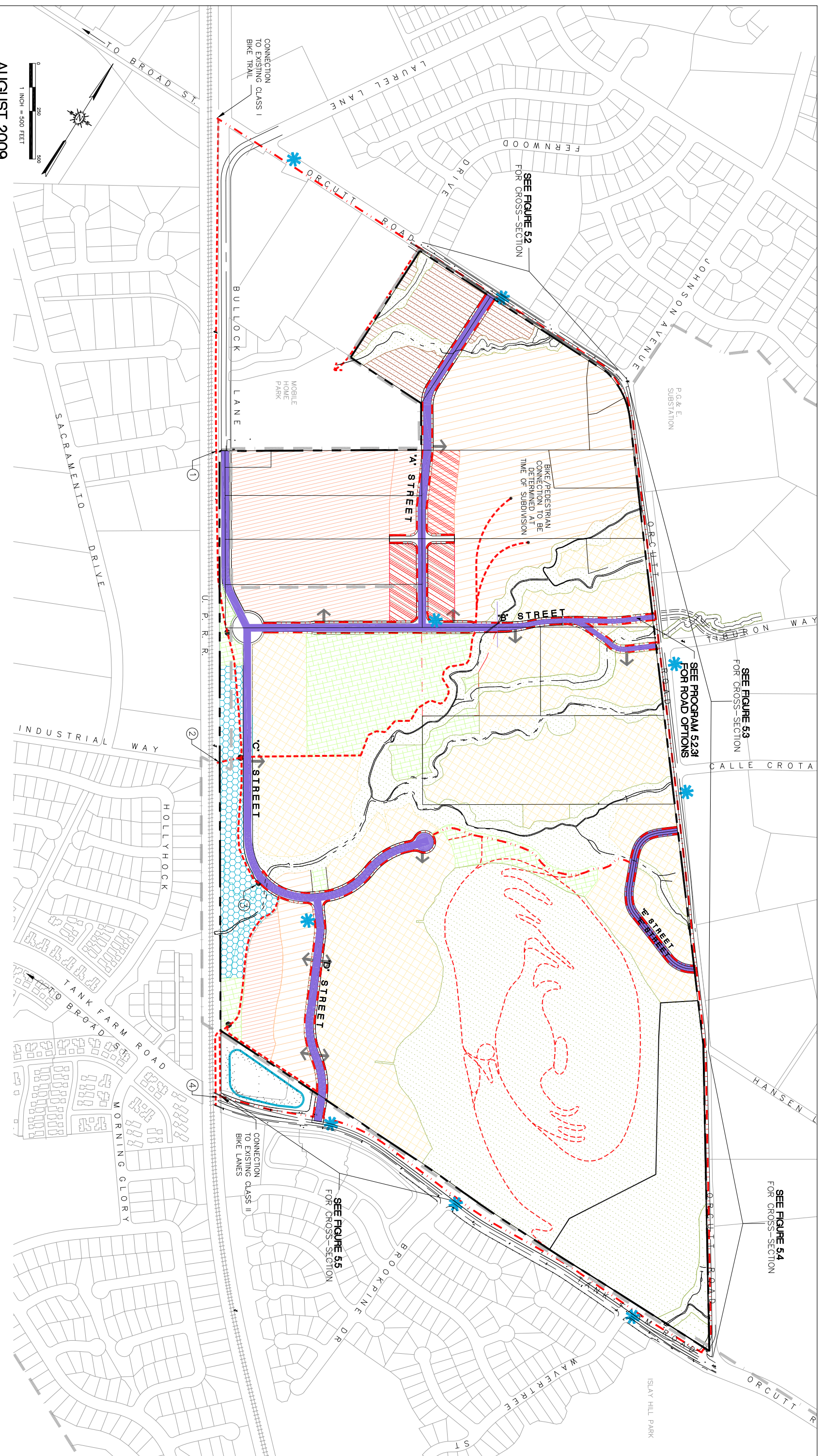
Tank Farm Road is four lanes between Broad Street and the proposed "D" Street intersection and two lanes between "D" Street and Orcutt Road. Based on the traffic study prepared for the Program EIR, traffic volumes are expected to increase by 2,378 ADT on Tank Farm Road at build-out as a result of the new development. However, given the existing number of lanes on Tank Farm Road, no additional through lanes are required but a turning lane will be provided.

Policy 5.1.a: *Existing arterial roadways should be improved where necessary in order to provide safe, adequate circulation.*

ORCUTT AREA SPECIFIC PLAN

5 Circulation

- Program 5.1.1: Orcutt Road shall be improved to include a continuous two-way left-turn lane, Class II bicycle lane, and curb and gutter between Johnson Avenue and Tank Farm Road (**Figures 5.2A-B, 5.3A-B, and 5.4**).
- Program 5.1.2: Tank Farm Road shall be widened at “D” Street, Brookpine Drive, and Wavertree Street to provide left-turn lanes. Widening of Tank Farm Road shall be consistent with City standards for Parkway Arterials which include installation of curb and gutter, sidewalks and bus pullouts (**Figure 5.5**).
- Program 5.1.3: Minor realignment of the Tank Farm Road/Orcutt Road intersection shall be completed in Phase I to correct the existing skewed alignment.
- Policy 5.1.b: Intersection separation between the existing roads and new Orcutt Area roads joining Orcutt Road and Tank Farm Road shall be no less than 150 feet and is recommended at 200 feet.*
- Policy 5.1.c: New individual driveway access onto Orcutt Road shall be prohibited under the Specific Plan. Existing driveways with access onto Orcutt Road may be used either for the existing number of units already constructed on land under one ownership or up to a total of two units for each existing ownership if not now constructed. Additional units above these thresholds shall require new road and intersection improvements as provided in this Specific Plan. Exceptions to this provision may be approved by the City Council for interim facilities approved as part of a subdivision phasing plan or development plan.*
- Policy 5.1.d: The City has determined that additional studies and funding from sources other than development in the Orcutt Specific Plan Area will be required for the UPRR over crossing. The costs and phasing of this project shall not delay the orderly progress of development within the Orcutt Area.*
- Policy 5.1.e: Applicants for projects within the Orcutt Area shall pay their fair share of circulation improvements, some of which may not be 100% attributable to development of the Plan area, into the Traffic Impact Fee Program as determined by the Director of Public Works.*
- Policy 5.1.f: City shall work to develop additional funding sources for bicycle bridge construction connecting to points outside the specific plan area.*



AUGUST 2009

ORCUTT EXPANSION AREA
SPECIFIC PLAN

LEGEND

	COLLECTOR		CLASS I BIKE PATH		R-1		OPEN SPACE
	BUS STOPS		CLASS II BIKE LANE		R-2		DETENTION BASIN AREA
	APPROXIMATE CONNECTION LOCATION OF ESSENTIAL LOCAL STREETS		RECREATION TRAIL		R-3		MIXED USE/ NEIGHBORHOOD COMMERCIAL/RESIDENTIAL
	CONNECTION POINT				R-4		PARKLAND

- NOTES**
1. CLASS ONE BICYCLE PATH ADJACENT TO ROAD DUE TO LACK OF RIGHT-OF-WAY
 2. LOCATION OF EIR DESIGNATED GRADE SEPARATED PEDESTRIAN/BICYCLE RAILROAD CROSSING
 3. CLASS ONE PATH CROSSES AT ROAD BRIDGE TO MINIMIZE WETLAND IMPACTS
 4. CLASS ONE PATH ON RAILROAD PROPERTY IS SHOWN FOR CLARITY PURPOSES, NOT A PART OF THE SPECIFIC PLAN RESPONSIBILITY.

FIGURE 5.1
CIRCULATION PLAN

FIGURE 5.2A ORCUTT ROAD FROM LAUREL LANE TO FERNWOOD DRIVE

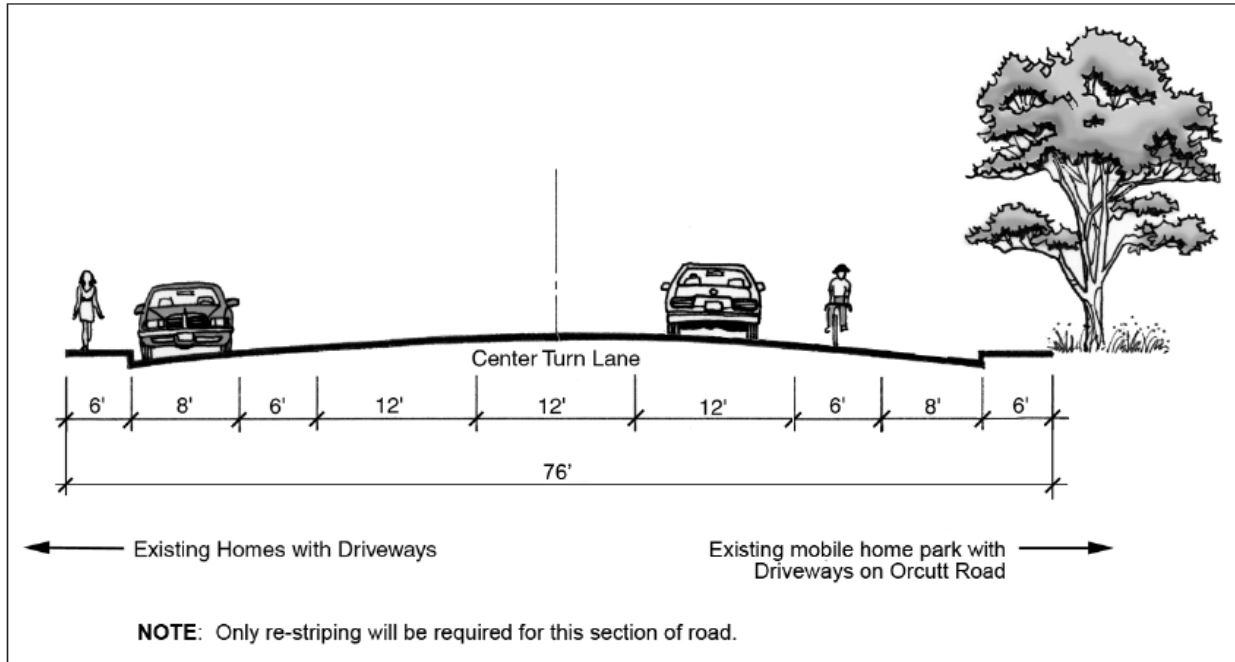


FIGURE 5.2B ORCUTT ROAD FROM FERNWOOD DRIVE TO JOHNSON AVENUE AT CULVERT/CREEK CROSSING OR SLOPES OVER 1:4

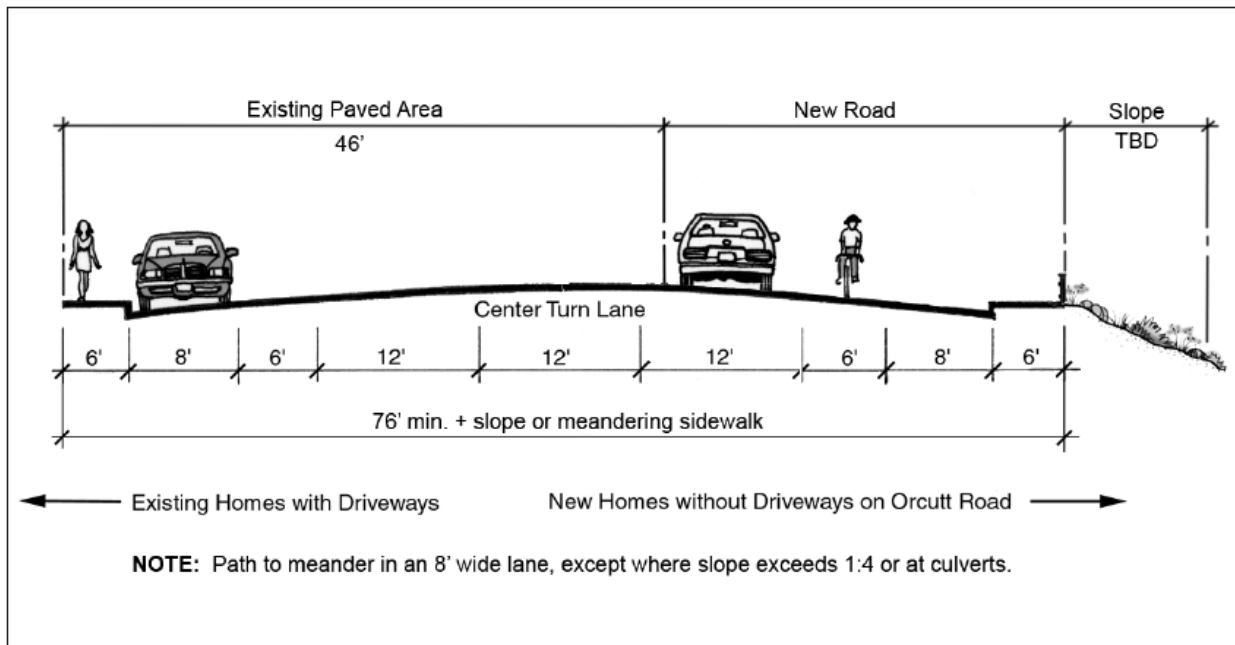


FIGURE 5.3A ORCUTT ROAD FROM JOHNSON AVENUE TO HANSEN LANE

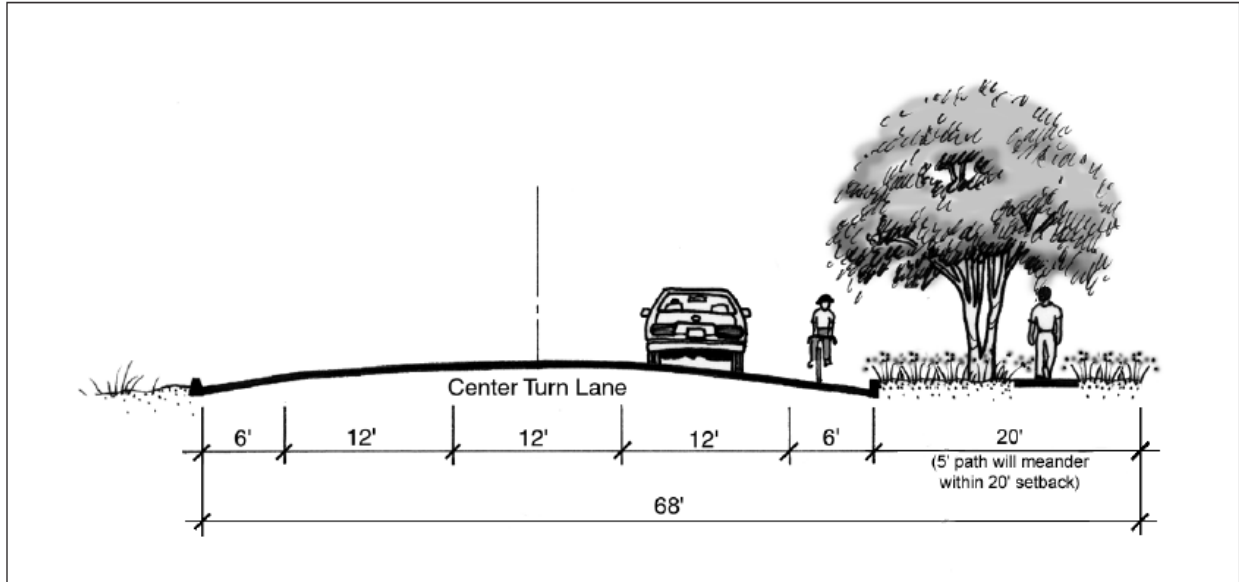


FIGURE 5.3B ORCUTT ROAD FROM JOHNSON AVENUE TO HANSEN LANE AT CULVERT/CREEK CROSSING

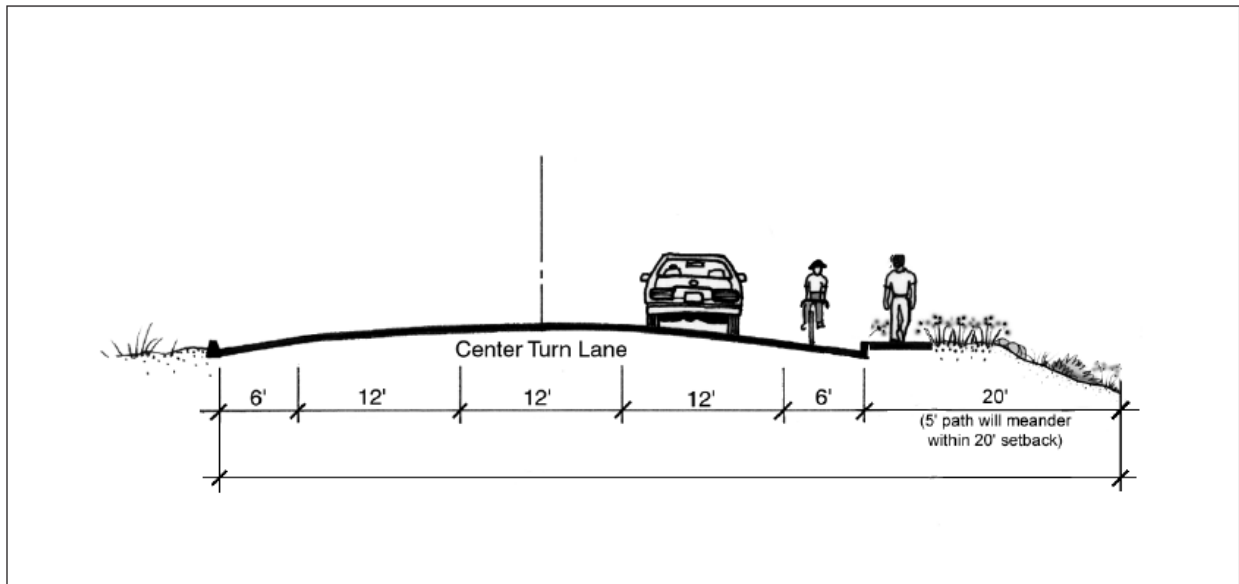


FIGURE 5.4 ORCUTT ROAD FROM HANSEN LANE TO TANK FARM ROAD (AFTER TERMINATION OF CENTER LANE ON ORCUTT ROAD)

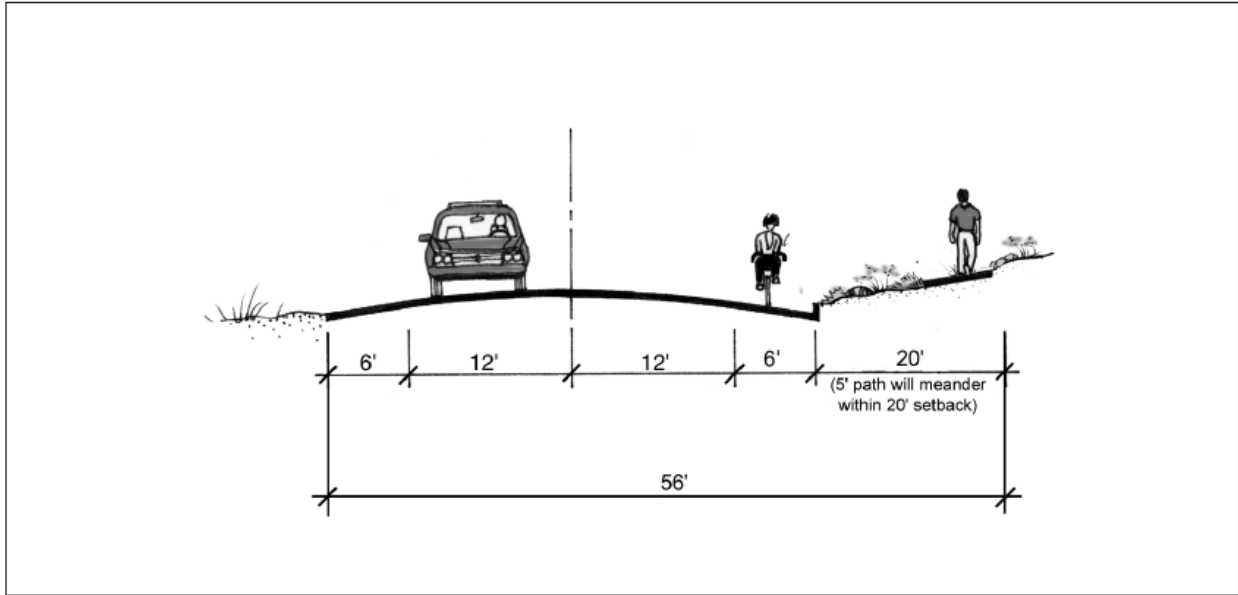
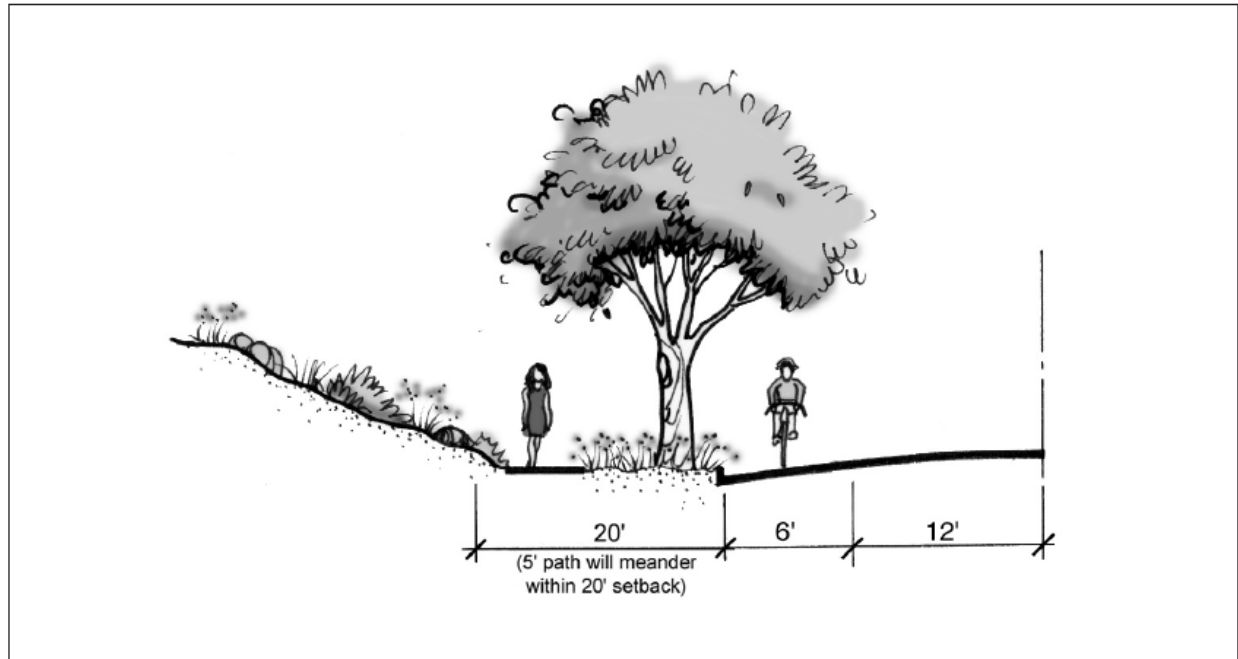


FIGURE 5.5 NORTH SIDE OF TANK FARM ROAD FROM RAILROAD TO BROOKPINE DRIVE



5.2 COLLECTOR STREETS

New residential collector streets, “A”, “B”, “C” (including the Bullock Lane extension), and “D” Streets, network through the Orcutt Area to connect the residential areas to Orcutt Road and Tank Farm Road. These streets are the connecting links for the residential areas.

Policy 5.2.a: Four new collector streets shall be constructed within the Orcutt Area to provide connections to existing arterials.

Policy 5.2.b: Features shall be incorporated into the circulation plan to promote traffic calming consistent with the City’s 25 mph standard in residential areas. The design of traffic calming devices will ensure adequate access and circulation for emergency vehicles and public transit and provide for underground utilities.

Program 5.2.1: All collector streets in the Orcutt Area will consist of a single lane of travel in each direction.

Street Specific Programs

Program 5.2.2: The entire “A” Street right of way shall include Class II bike lanes, and separated sidewalks on both sides of the street. No street parking will be allowed on “A” Street except along the neighborhood commercial frontage (**Figure 5.6**).

Program 5.2.3: The “B” Street right of way shall be developed as follows:

- a) Orcutt Road to Neighborhood park right of way shall include separated sidewalk, Class II bike lanes, and street parking on the north side of the street only, similar to **Figure 5.7**.
- b) Neighborhood park frontage shall include Class II bike lanes, and separated sidewalks on both sides of the street, except at the commercial area (refer to design concept plan). Street parking will be allowed on the south side of the street only (**Figure 5.7**).
- c) The neighborhood park to the traffic circle right of way shall include Class II bike lanes, and separated sidewalks on both sides of the street. No street parking will be allowed along this portion of “B” Street (**Figure 5.6**).
- d) Traffic calming features on “B” Street include landscaped bulb-outs at the “A” Street intersection, street trees along planting strips and textured paving for crosswalks (**Figure 5.8**).
- e) A pedestrian friendly intersection with “A” Street has been identified in the design section of the OASP. Specific improvements call for traffic calming paving and street alignment, special landscape features and urban amenities.
- f) Engineering review of the Specific Plan identified that “B” street must connect through to Orcutt Road either with a direct connection at Tiberon Way to the east, or with a 250 foot offset. Several alternatives for the direct connection exist including one that requires taking of property to the east of Orcutt and relocating Tiberon Way. Two of the most viable options for the intersection of “B” Street to Orcutt Road are shown on the Specific Plan map. The actual option selected will depend either on which property owner with requirements to connect to Orcutt Road develops first, or that a mutually satisfactory resolution to the alignment is reached between the adjacent property owners. The less costly and preferred alignment for providing both a standard intersection with Tiberon Way and allowing street access to the southeast parcel is option “A”. However, if this

owner does not annex and develop, then the owner of the northeast parcel shall have the right to exercise option “B” or pursue alternative means to implement option “A.”

- Program 5.2.4: “C” Street right of way shall be developed as follows:
- a) Traffic circle to “C” Street Bridge right of way shall include separated sidewalk. No street parking shall be allowed along his section of “C” Street (**Figure 5.9**).
 - b) “C” Street Bridge to Righetti Hill open space right of way shall include Class II bike lanes on both sides of the street. No street parking will be allowed along this portion of “C” Street (**Figure 5.6**). (A separated Class I bicycle path is provided within the linear park.)
 - c) Traffic calming on “C” Street will be achieved through reduced sight distance with gently curving alignments.
- Program 5.2.5: The entire “D” Street right of way shall include Class II bike lanes, and separated sidewalks on both sides of the street. No street parking will be allowed on “D” Street. Traffic calming on “D” Street will be achieved through reduced sight distance with gently curving alignments (**Figure 5.6**).
- Program 5.2.6: Existing Bullock Lane will be extended south to connect with the traffic circle at the intersection of "B" and "C" Streets. The right-of-way shall include street parking and adjoining sidewalks (**Figure 5.10**).
- Program 5.2.7: A traffic circle should be constructed at the intersection of “B” and “C” Streets and Bullock Lane which will allow for safe flow of traffic through the 3-way intersection while promote traffic calming by slowing driving speeds.
- Program 5.2.8: Shared driveway access for multi-family developments and single-family residences will be allowed on “A”, “B”, “C”, or “D” Streets. Private driveway access for single family residences will not be allowed on most of “A”, “B”, “C”, or “D” Streets with the exception of limited private driveway access allowed on:
- a) The eastern portion of “B” Street from the “B” Street Bridge east to Orcutt Road.
 - b) “C” Street in the residential areas east of the “C” Street Bridge.

FIGURE 5.6 COLLECTOR: 'A' STREET, 'D' STREET, AND A PORTION OF 'B' AND 'C' STREETS (EXCEPT AT MIXED-USE AREA, SEE FIGURE 3.1)

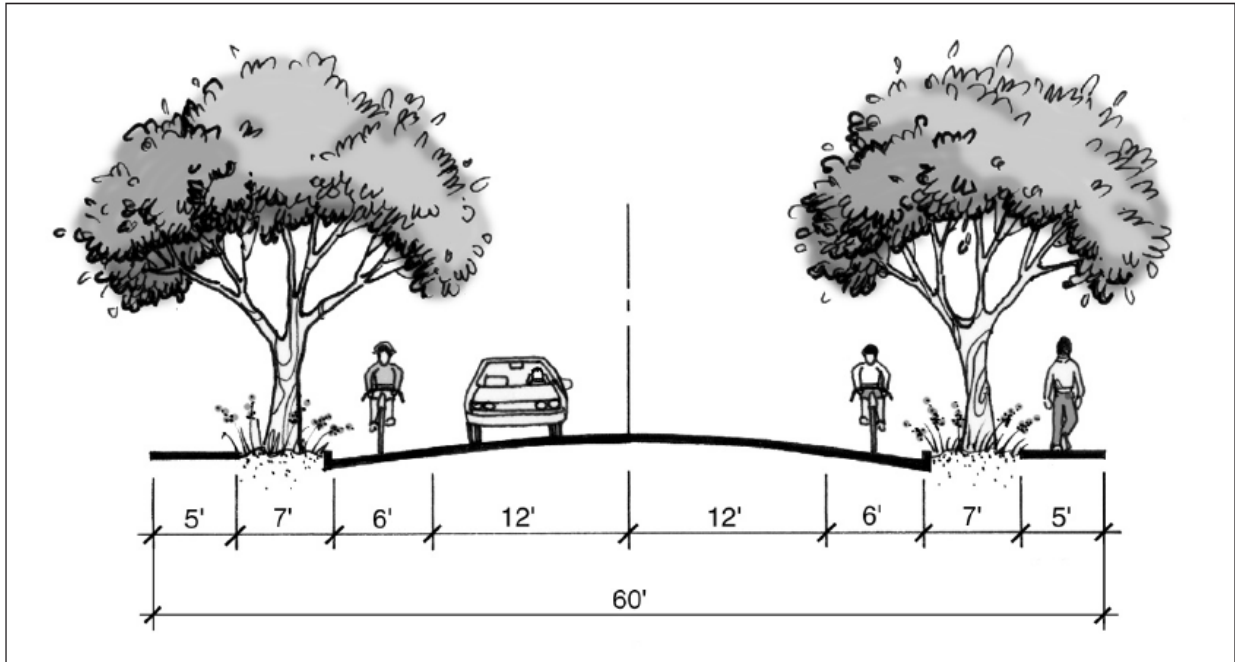


FIGURE 5.7 COLLECTOR: 'B' STREET FRONTING NEIGHBORHOOD PARK

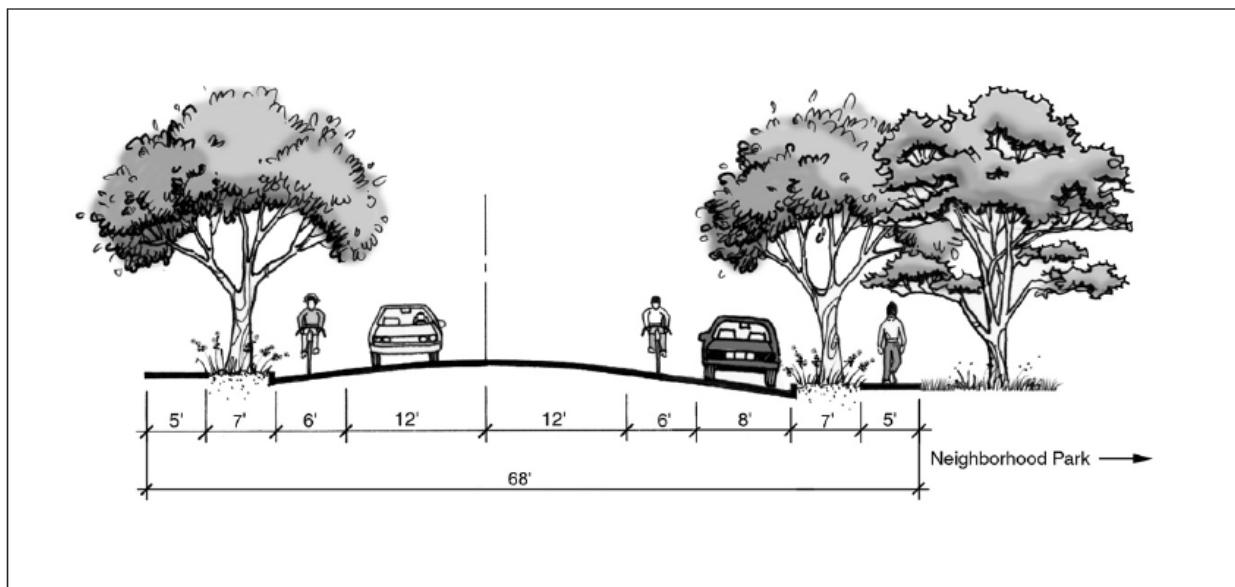


FIGURE 5.8 TRAFFIC CALMING FEATURES ON 'B' STREET

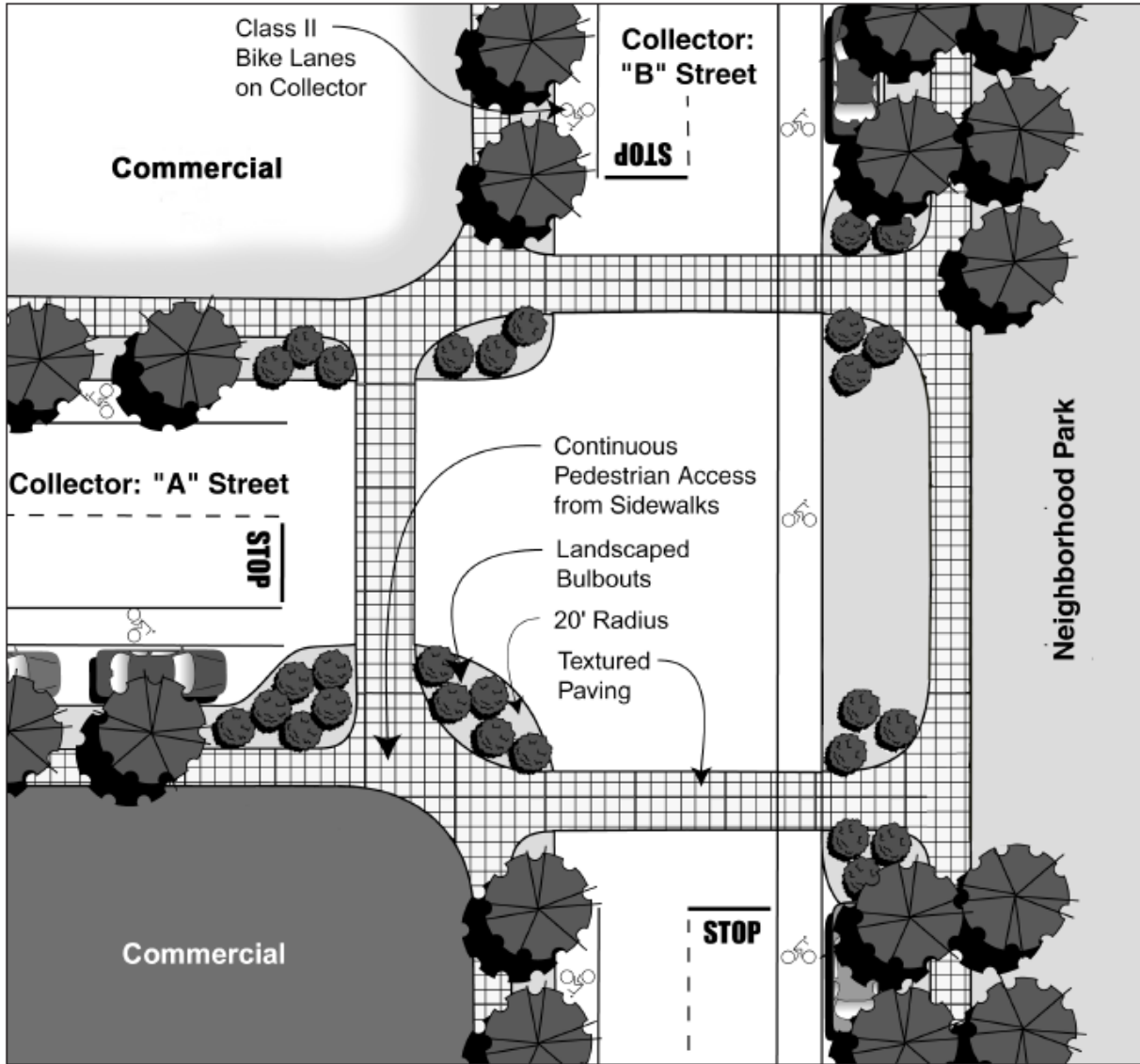


FIGURE 5.9 COLLECTOR: 'C' STREET FROM TRAFFIC CIRCLE TO BRIDGE

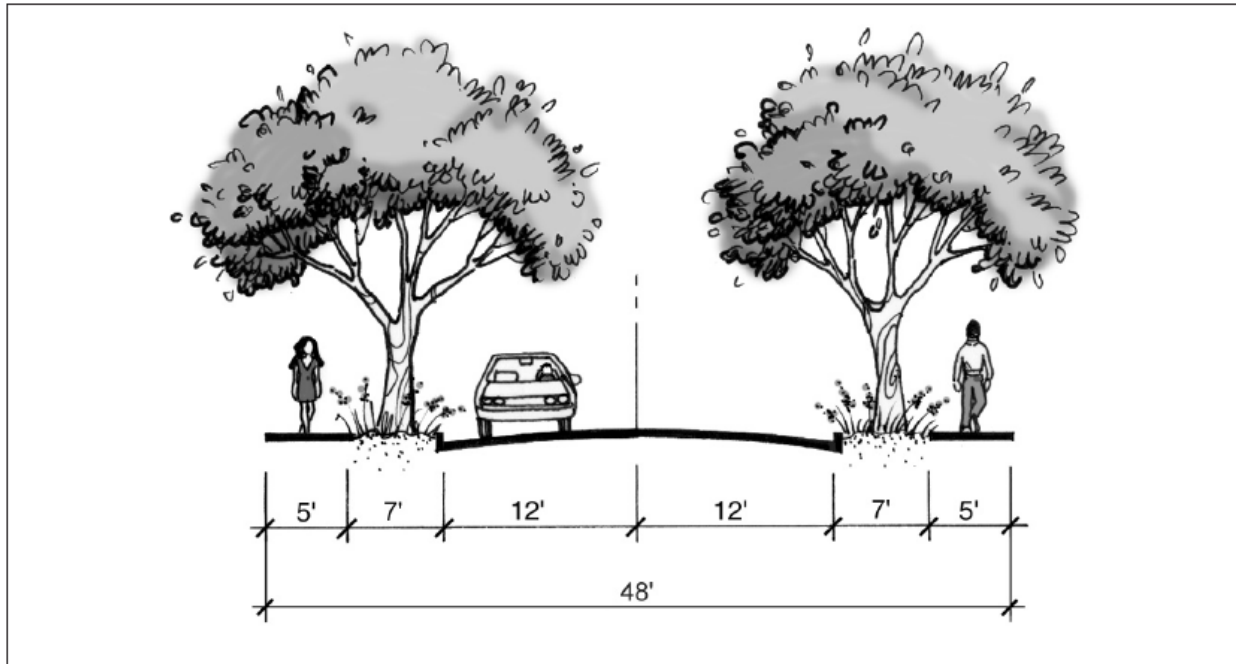
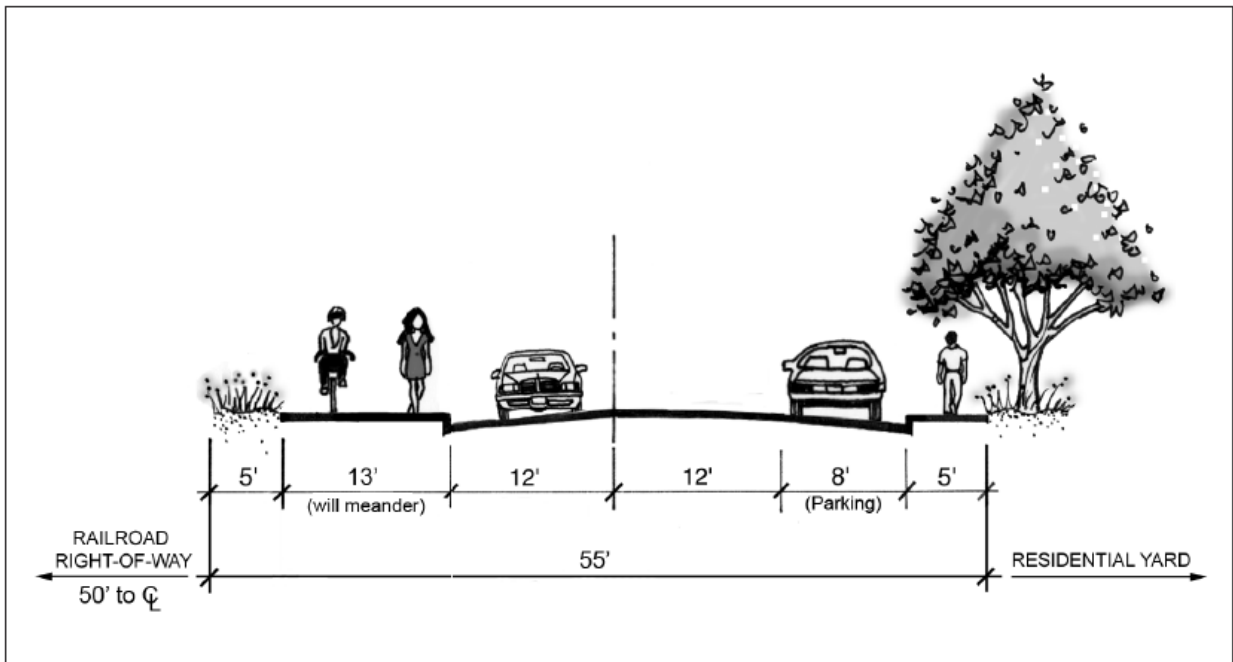


FIGURE 5.10 COLLECTOR: BULLOCK LANE



5.3 LOCAL STREETS

The function of local streets is to move traffic from the residential areas to collector streets. While exact locations and layout of local streets are not established by the Specific Plan, certain connection points for local streets have been identified which are necessary to provide for adequate property access and interior neighborhood circulation.

*Policy 5.3.a: The points of connection of local streets indicated in **Figure 5.1** shall be included in the engineering design of the local streets layout in residential developments. The exact location of local streets points of connection may vary from the location shown provided that the intent of the connection is met.*

Policy 5.3.b: Detailed layout of local streets shall be designed and constructed as development occurs and will adhere to standards identified in the street sections included in the Specific Plan.

*Policy 5.3.c: Curbside parking and private driveway access shall be permitted on local streets. A cross-section of a typical local street is shown in **Figure 5.11**.*

Policy 5.3.d: Alleys are encouraged to facilitate access to residential lots and to improve the appearance of local streets.

Program 5.3.1: “E” Street right of way shall be developed as follows:

“E” Street may be developed with a loop configuration instead of a cul-de-sac, subject to a detailed evaluation of the intersection location with Orcutt Road during the Subdivision Review process.

From residential area/Righetti Hill open space trailhead to Orcutt Road right of way shall include Class III bike lanes, and adjoining sidewalks on both sides of the street. Street parking shall be permitted along this portion of “E” Street (**Figure 5.12**).

Program 5.3.2 Where private alleys are desirable to improve the access to residential lots, allow the area of the alley to count towards net site area for determining allowable density.

5.4 PUBLIC TRANSPORTATION

The City currently provides daily public transportation to the Orcutt Area via Route 3. Route 3 follows Broad Street to Tank Farm Road, and then continues on Orcutt Road to the downtown area. Routes 1 and 8 provide access to Orcutt Road between Broad Street and Johnson St during the week and the school year respectively.

Goal 5.4: User friendly public transit service in the Orcutt Plan Area that is safe and facilitates the City’s public transit goals and programs.

Policy 5.4.a: Provide bus routes and bus stops within the Plan Area for inclusion on existing City bus routes or future bus routes.

Programs 5.4.1: Final location of bus routes, bus stops, and bus pullout areas will be determined in conjunction with the City transit authority at the time of development approvals. **Figure 5.1** provides suggested bus route and bus stop locations to serve the Orcutt Plan Area.

FIGURE 5.11 LOCAL STREETS IN RESIDENTIAL AREAS

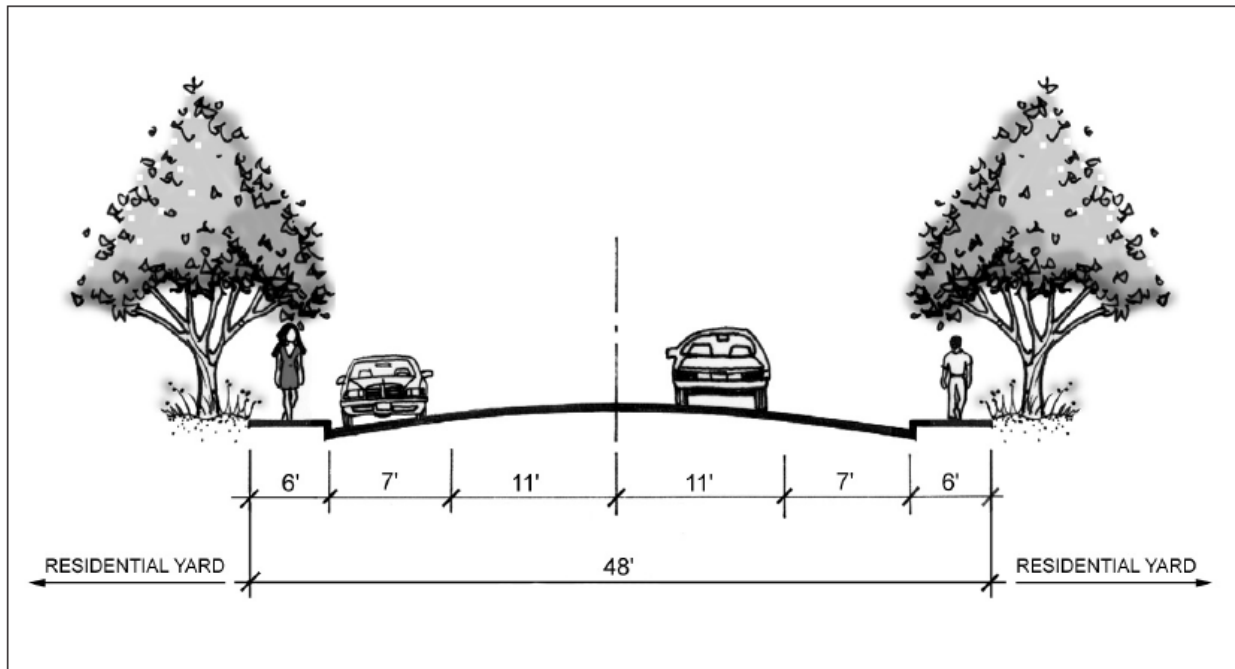
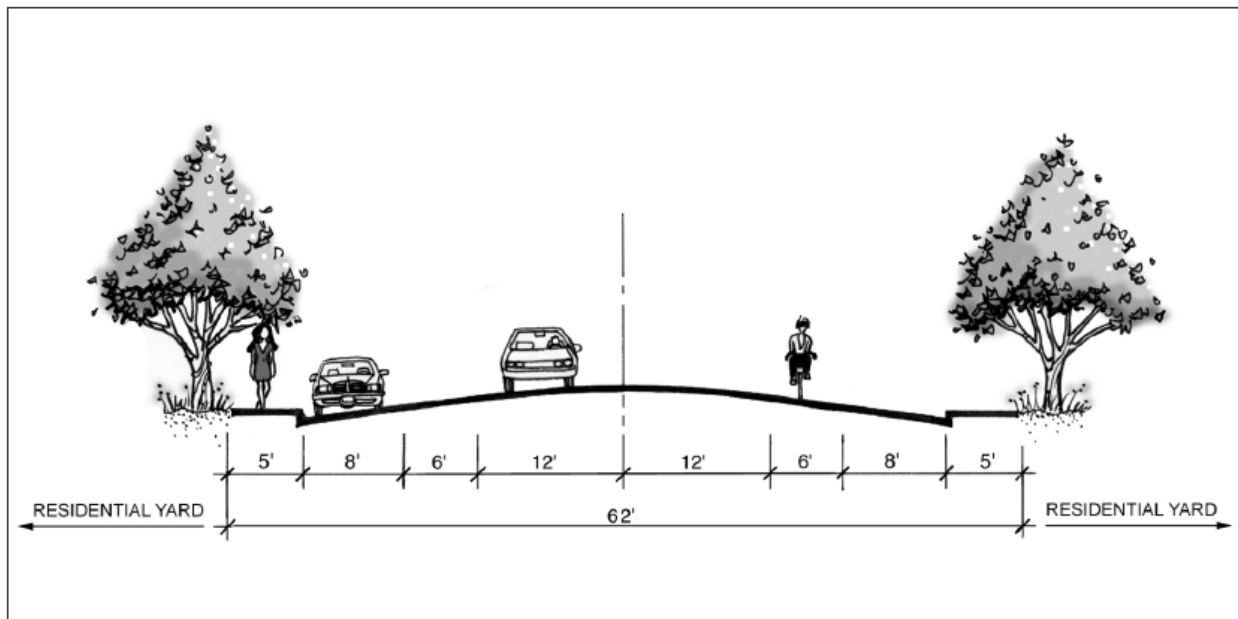


FIGURE 5.12 COLLECTOR: 'E' STREET THROUGH EASTERN RESIDENTIAL AREA



5.5 PEDESTRIAN AND BICYCLE PATHS

Consistent with the Circulation Element's goal of promoting alternative modes of transportation this Specific Plan includes bicycle and pedestrian circulation routes which provide access throughout the interior of the Orcutt Area and connect to the existing pedestrian and bicycle network outside the Orcutt Area. Pedestrian circulation is provided along arterial, collector, and local streets in the Orcutt Area.

Goal 5.5.: **A safe and user friendly bicycle circulation system within the Specific Plan Area.**

Policy 5.5.a: *All Class I bicycle/pedestrian paths will utilize paving materials for an all-weather surface. **Figure 5.13** illustrates a typical cross-section for all proposed Class I bicycle paths.*

Policy 5.5.b: *Bicycle path development standards in the City Bicycle Transportation Plan shall apply unless superceded by standards set forth in this Specific Plan.*

Policy 5.5.c: *Bicycle path width, paving, signs and other features shall be as specified in the Bicycle Transportation Plan.*

Policy 5.5.d: *Construction of bicycle paths and lanes through various portions of the Orcutt Area will be concurrent with phasing of development construction. (**Figure 10.2 and 10.3**)*

Class I Bike Paths

Policy 5.5.e: *A north-south Class I bicycle path parallel to the railroad shall connect the Class I Railroad Bicycle path and Class II bike lanes on Orcutt Road (at Laurel Lane) to the north with Class II bike lanes on Tank Farm Road to the south (**Figure 5.1**).*

Policy 5.5.f: *Provision will be made for the continued connection of the north-south Class I bicycle path from the southeast of "C" Street bridge to the railroad bridge at Tank Farm Road for a connection to the Arbors area should the bicycle path be extended by the City to the Arbors neighborhood via the railroad bridge.*

Program 5.5.1: The north-south bicycle route will connect with existing Class II bike lanes along Orcutt Road and Tank Farm Road. The Class I bicycle path through the linear park will run parallel to the railroad tracks from the "C" Street Bridge to Bullock Lane. From Bullock Lane the Class I bike path will continue parallel with Bullock Lane and the railroad tracks, north to the Orcutt Road/Laurel Lane intersection to connect with the Railroad Bicycle Path and Class II lanes on Orcutt Road. (**Figures 2.1 and 5.1**) The Class I bike path through the linear park will connect with "C" Street to utilize the roadway bridge in crossing the creek. The roadway bridge will provide a separated pedestrian/bike lane. From the "C" Street Bridge to Tank Farm Road Class II bike lanes on "D" Street will be provided to complete the north-south connection (See Policy 5.5.6 above). The length of the Class I bike path from Orcutt Road through the linear park to the creek will be constructed in Phase I. The Class I path through the linear park will also connect with the Class I bicycle path through the neighborhood park at the "C" Street bridge.

Policy 5.5.g: *A Class I bicycle path shall provide a single east-west connection through the Orcutt Area (**Figure 5.1**).*

Program 5.5.2: The Class I bike path through the neighborhood park will follow the course of the creek corridor from "C" Street north through the park to "B" Street. At "B" Street it shall become a Class II path which in turn connects to the Class II lanes at the Orcutt Road intersection. (**Figure 5.6**)

Class II Bike Lanes

Policy 5.5.h: Class II bicycle lanes shall be provided along Orcutt Area collector streets, Orcutt Road and Tank Farm Road..

Program 5.5.3: **Figure 5.5** shows a cross-section of Tank Farm Road with a 6-foot-wide Class II bicycle lane, the landscaped buffer, and sidewalk added along the north side of the road. **Figures 5.2, 5.3** and **5.4** show cross-sections of Orcutt Road with the Class II bicycle lane landscaped buffer and separated pedestrian path along the west side of the road. The draft OASP includes a bridge over the railroad tracks at Industrial Way that would also be used by bicyclists and pedestrians to access Broad Street and Marigold Center.

5.6 CIRCULATION STANDARDS

Established City standards apply unless superceded by specific standards in this Specific Plan. Lane widths, surfaces, signage, lighting, curb, gutter, sidewalks and driveways shall be as specified in the City’s Circulation Element. City standards for hillside streets will be used for all streets on hillsides. However reduced street sections are encouraged in residential areas and to increase the density of development so that the number of residential units anticipated in the Specific Plan can be achieved.

Policy 5.6: Reduced and varied street widths are encouraged in residential areas to provide greater design flexibility, and increase housing density. Modifications to City street standards will be considered on a case by case basis at the time of subdivision submittal and where such modifications are acceptable to the City Fire Marshall and Public Works Director. Street width can be reduced by removing on-street parking. Using Class III instead of Class II bike lanes, using alleys to access on-site parking, reducing sidewalk and parkway widths and by creating private streets.

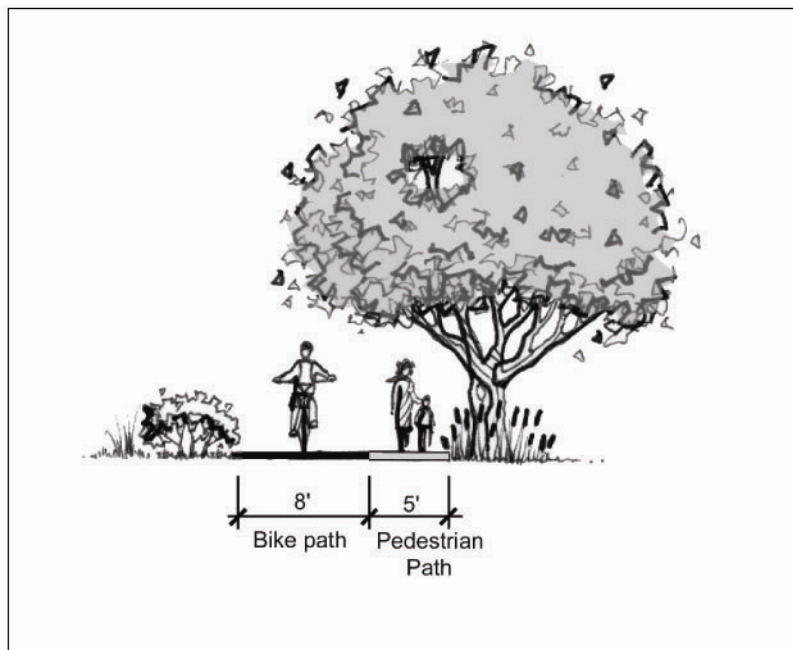


FIGURE 5.13 BICYCLE AND PEDESTRIAN PATHS

6 PUBLIC UTILITIES

Public utilities include potable and recycled water lines and supply, wastewater facilities, storm water facilities and utilities such as natural gas, electrical, telephone, and cable service. Developers in the Orcutt Area will pay City established water and wastewater development impact fees which will fund infrastructure necessary to support development in the Orcutt Plan Area. All on-site infrastructure will be the responsibility of the developer. Specific project approvals (i.e. building permits) can be issued only when adequate wastewater, water supply, and storm water facilities and supplies are available for the proposed development. Adjacent landowners shall coordinate with each other and City staff to co-locate new utilities infrastructure through shared utility corridor easements or dedication of public rights-of-way.

6.1 WATER SUPPLY

Currently, water to the majority of properties in the Orcutt Area is provided by wells on the individual properties. Several properties near the intersection of Orcutt Road and Johnson Avenue and fronting on Bullock Lane are already served by City water lines. New 12-inch diameter public water mains will be constructed along the proposed residential collectors, “A”, “B”, “C”, and “D” Streets. These lines will connect to the 8-inch and 12-inch water mains along Bullock Lane, Orcutt Road, and Tank Farm Road to provide a looped water supply (**Figure 6.1**). Water for the Righetti Ranch home open space area will be provided by a connection to the existing water line in Tank Farm Road. Pressure regulating valves or other appurtenances may be needed as a part of the required water system improvements to be certain that the new area interacts properly with the existing water system.

Based on the proposed units of residential and commercial development and the City’s water-use factors, proposed development in the Orcutt Area is anticipated to require approximately 220 acre-feet per year of potable water for residential and commercial uses (including private landscaping). An additional estimated 30 to 40 acre-feet of water would be used for landscaping for common outdoor areas in the multi-family residential developments and for commercial and public landscapes such as the Neighborhood Park and linear park. These areas could utilize reclaimed water. Any irrigation needed to establish or maintain vegetation in the storm water detention and riparian enhancement areas will be required to use recycled water.

Potable water for the Orcutt Plan Area will be supplied from the City via the existing City supply and new City supply from water projects as may be designated. The new development will incorporate water conservation features such as low-flow faucets and showerheads, drought-tolerant landscaping, and drip irrigation systems. Non-potable water for public landscaping will be provided through the City’s water reuse project. Point of connection to the City’s water reuse system and location of the reclaimed water main in the Orcutt Area are shown in **Figure 6.1**. The new development will also utilize a dual-water system to allow use of City recycled water for public landscaping in the parks, landscaped buffers, the community commercial mixed use area, and common outdoor areas in the multi-family residential areas. Commercial mixed use and multi-family development projects will include reclaimed water irrigation systems in their landscaping plans. The initial phase of development may be constructed using a combination of reclaimed water, and well water. Full development of the Orcutt Area will require additional water supply sources to be available through City water supply projects.

Special care shall be given to all development plans, especially when recycled water will be implemented in phases or in conjunction with the use of well water, to ensure that all regulations regarding the use of recycled water are met and maintained. One of these regulations prohibits the use of recycled water within 50 feet of any well.

The City’s goals to maximize the use of recycled water may result in requirements to install recycled water mains beyond the limits shown in **Figure 6.1**. Such recycled water main extensions may be eligible

for reimbursement by the City and/or future development in the area, when the extensions are required to be installed beyond that needed to serve the proposed development.

6.2 WASTEWATER FACILITIES

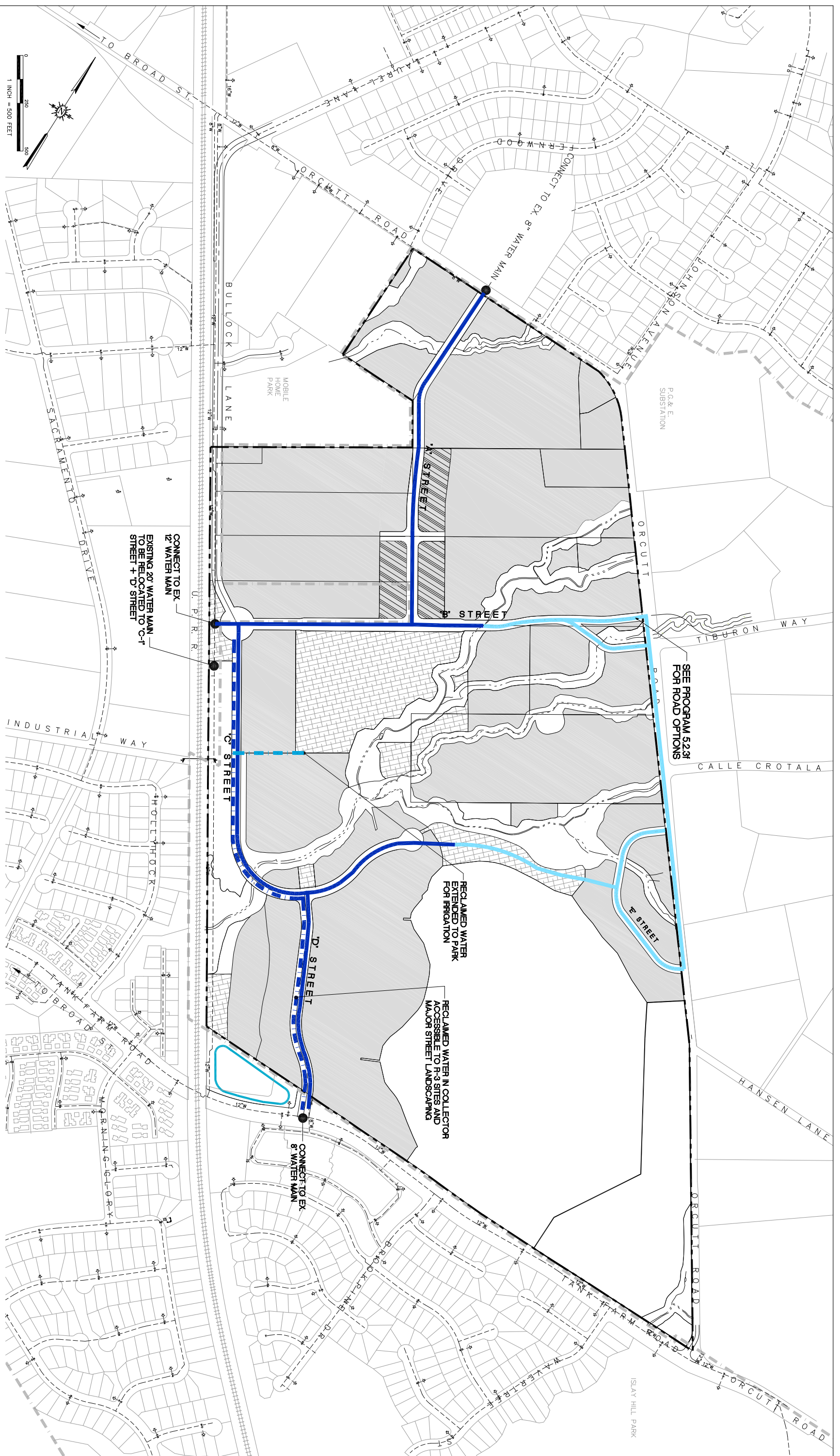
Individual septic tanks currently provide wastewater treatment for the majority of properties in the Orcutt Area. Wastewater from the area covered by this specific plan will be conveyed to a new sewer that will cross under the railroad at Industrial Way. Wastewater will then be conveyed down Industrial Way to a new 10" sewer in Broad Street. See **Figure 6.2** for existing and proposed sewer service lines in the Orcutt Area. Alternatives to this configuration may be considered by the City in order to avoid a new railroad crossing, if it can be demonstrated that adequate capacity is available in existing sewer mains near the Orcutt Area. The completed development will generate an estimated 149,000 gallons/day of wastewater from the residential area and 2,000 to 3,200 gallons/day from the mixed-use area. This estimate is based on the City's standard wastewater generation rate of 190 and 120 gallons per day/unit for single-family and multi-family residential uses, respectively and 0.20 gallons per day/square foot of commercial space. The City's wastewater treatment plant is nearing capacity, and planning has begun on an upgrade project that will meet the needs of General Plan build-out. It is expected that the capacity improvements will be in place prior to any demand for that additional capacity. Depending on the timing of needed improvements relative to the pace of development and construction in the City, however, a temporary resource deficiency could occur. If any particular project results in a demand that would exceed available capacity at the wastewater treatment plant, building permits could be delayed until the needed capacity is available. The current project schedule indicates that improvements for build-out capacity at the City's Water Reclamation Facility will be completed by 2010. The cost of providing the additional capacity will be incorporated into the City's Wastewater Impact Fee structure. A pre-annexation agreement will establish criteria for when any existing buildings will be required to connect to the sewer system and pay the associated Wastewater Impact Fee.

6.3 STORM WATER FACILITIES

The Orcutt Planning Area is located within the watershed of the East Branch of San Luis Obispo Creek. Drainage features on the site include seven small perennial streams, five of which join together mid-site into one channel. The site has two distinct drainage sub-areas. The Upper Fork East Branch San Luis Obispo Creek Watershed (UFEBSCO) includes the southeastern 155.3 acres of the Specific Plan area and drains to the southwest into the east branch of the San Luis Obispo Creek. The Orcutt Creek Watershed includes the northwestern 10.4 acres of the Specific Plan area and drains to the southwest into Orcutt Creek. Both creeks ultimately are tributary to San Luis Obispo Creek. A review of the Federal Flood Insurance Rate Map (FIRM) showing floodplains and flood hazard classifications indicated that the Orcutt Area has no Flood Zone 'A' or 'B' areas ('A' indicates the areas of 100-year inundation).

The proposed Drainage Master plan for the Orcutt Area meets the City's existing requirements for storm water management in new developments and complies with the City's Waterway Management Plan (WMP). The project also incorporates best management practices for stormwater quality control.

The San Luis Obispo Waterway Management Plan (WMP) sets forth criteria for drainage design for projects tributary to San Luis Obispo Creek. The drainage plan proposed for the Orcutt Area includes detention basins to detain storm water generated by development within UFEBSCO Creek and Orcutt Creek watersheds. The largest basin serves several of the ownership areas and the cost and maintenance will be shared accordingly. The Shared Basin is immediately adjacent to an existing basin serving the existing Arbors development project across Tank Farm Road. The drainage plan includes an option of



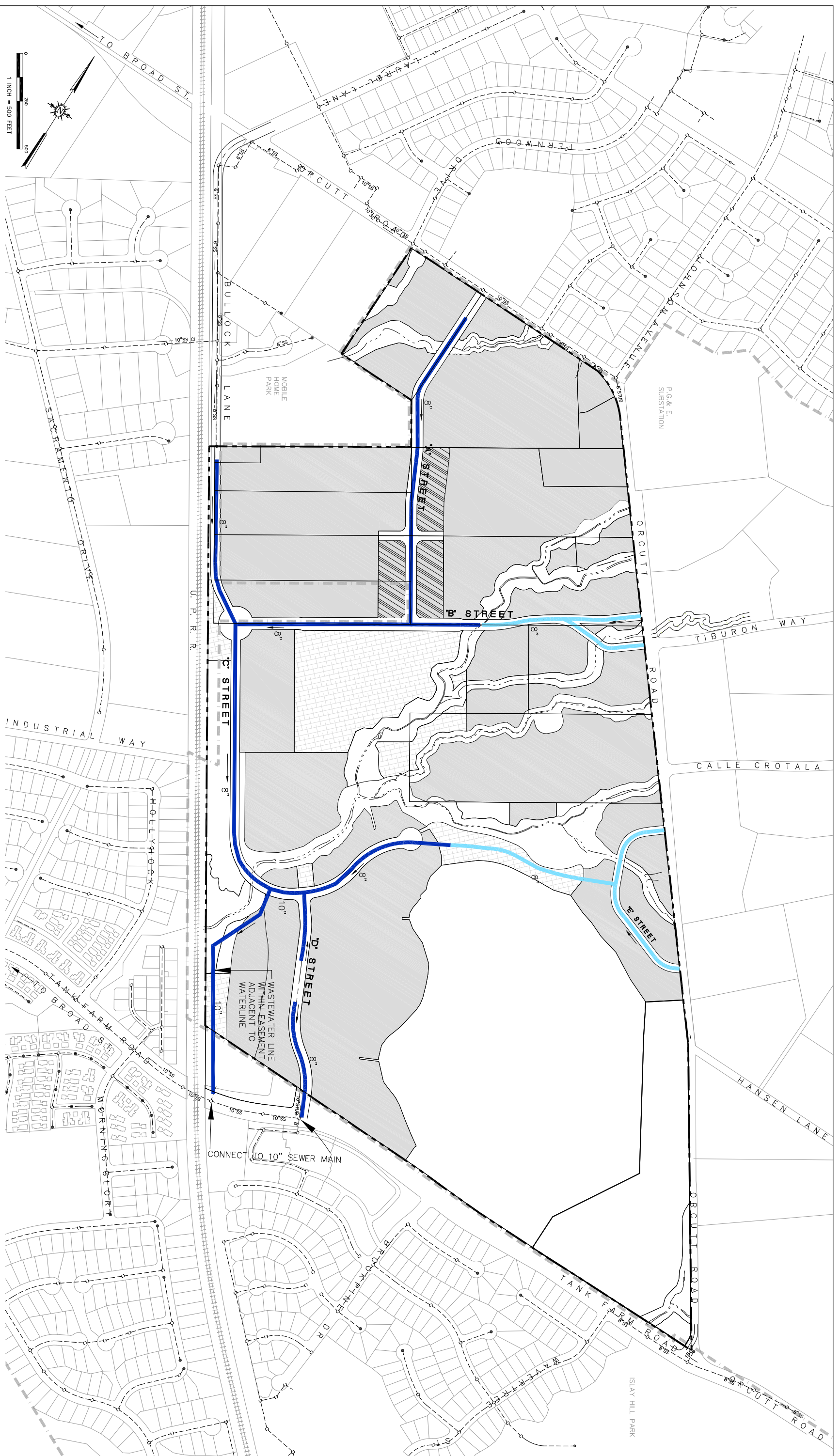
AUGUST 2009
 1 INCH = 500 FEET
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ORCUTT EXPANSION AREA
 SPECIFIC PLAN

LEGEND

- EXISTING WATER LINE
- PROPOSED RECLAIMED WATER MAIN-PHASE 1
- PROPOSED RECLAIMED WATER MAIN-PHASE 2
- PROPOSED WATER MAIN-PHASE 1
- PROPOSED WATER MAIN-PHASE 3
- RESIDENTIAL
- PARKLAND
- MIXED USE / NEIGHBORHOOD COMMERCIAL/RESIDENTIAL

FIGURE 6.1
WATER SUPPLY PLAN



AUGUST 2009

LEGEND

- EXISTING SEWER MAIN
- PROPOSED SEWER MAIN-PHASE 1
- PROPOSED SEWER MAIN-PHASE 3
- RESIDENTIAL
- PARKLAND
- MIXED USE/ NEIGHBORHOOD COMMERCIAL/RESIDENTIAL

ORCUTT EXPANSION AREA
S P E C I F I C P L A N

FIGURE 62
WASTEWATER PLAN

combining it with the existing Arbors Basin as discussed in more detail below. The drainage plan is shown on **Figure 6.3**. Major features of the plan are discussed below.

The lower portion of the UFEBSLO, as shown in **Figure 6.3**, is categorized as a “Secondary Waterway” according to watershed size criteria in the WMP. All other channels within the Specific Plan are categorized as Minor Channels. The grading plans and creek plans that affect the Secondary Channel will adhere to the WMP recommendations to establishing a constructed natural channel. These criteria are intended to result in a creek channel that has increased habitat value as well as adequate hydraulic capacity and stability.

Storm water runoff quality will be addressed for both Construction and Post-Construction phases of the project. Sediment control during construction will be important and a detailed Storm water Pollution Protection Plan (SWPPP) will be prepared for each grading project over 1 acre in disturbance. Construction Phase impacts will be addressed by the implementation of Best Management Practices (BMP’s) during construction, by the preparation and implementation of Stormwater Pollution Prevention Plans (SWPPP’s) and erosion control plans.

The Specific Plan also will utilize BMP’s for post construction stormwater quality. The primary method of treating stormwater quality will utilize bio-filtration. Bio-filtration is the most effective long-term method for improving stormwater runoff quality. These facilities will be located where the soil conditions are appropriate and locate to treat runoff containing pollutants (i.e. oils and or sediments) where the site design is compatible. Types of facilities that may be used in different site locations include bio-swailes, detention basin forebays (which may also be designed to function as a floodable terrace). Runoff from parking lots and outdoor storage areas should be treated as close to the source as possible. Bio-filtration facilities should be located near these sources of runoff, or the site should include pervious surfaces so that runoff and pollutants are minimized. Buffers on streams and wetlands will also be designed to optimize infiltration and minimize flooding impacts.

After development, much of the Orcutt Area will remain in open space and parks and storm water runoff from those areas will be relatively unchanged. However, development areas will generate additional surface runoff during storms. The WMP indicates that storm water detention should be used in areas where there are downstream capacity limitations, and where detention analysis indicates that it would be beneficial. Storm water detention basins are proposed for the Orcutt Area, consistent with the surrounding Edna/Islay developments and the recommendations of the *1999 Airport Area Storm Drainage Master Plan (AASPDMP)*.

Detention basin requirements are typically stated in terms of reducing the peak rate of runoff from a certain post-development storm, to the rate of a certain pre-development storm. Further, the Regional Water Quality Control Board (RWQCB) is considering future policies that may involve the allowed rate of release and volume of release. Within the surrounding area, detention standards have varied over time and are summarized as follows:

- Edna/Islay: Reduce 50-year post development runoff to 2-year pre-development rate.
- Airport Area: Reduce 100-year post development runoff to 10-year pre-development rate.
- City WMP: Detain if necessary to avoid impacts to problem areas for a range of storms 2 through 100 yr.
- Future: RWQCB consideration of hydromodification policies may result in detention standards.

Considering the above, the basins proposed in the Specific Plan utilize the following criteria to be consistent with the WMP, the hydrological study, and to be compatible with the surrounding area drainage, applied at the basin outlet.:

ORCUTT AREA SPECIFIC PLAN

6 Public Utilities

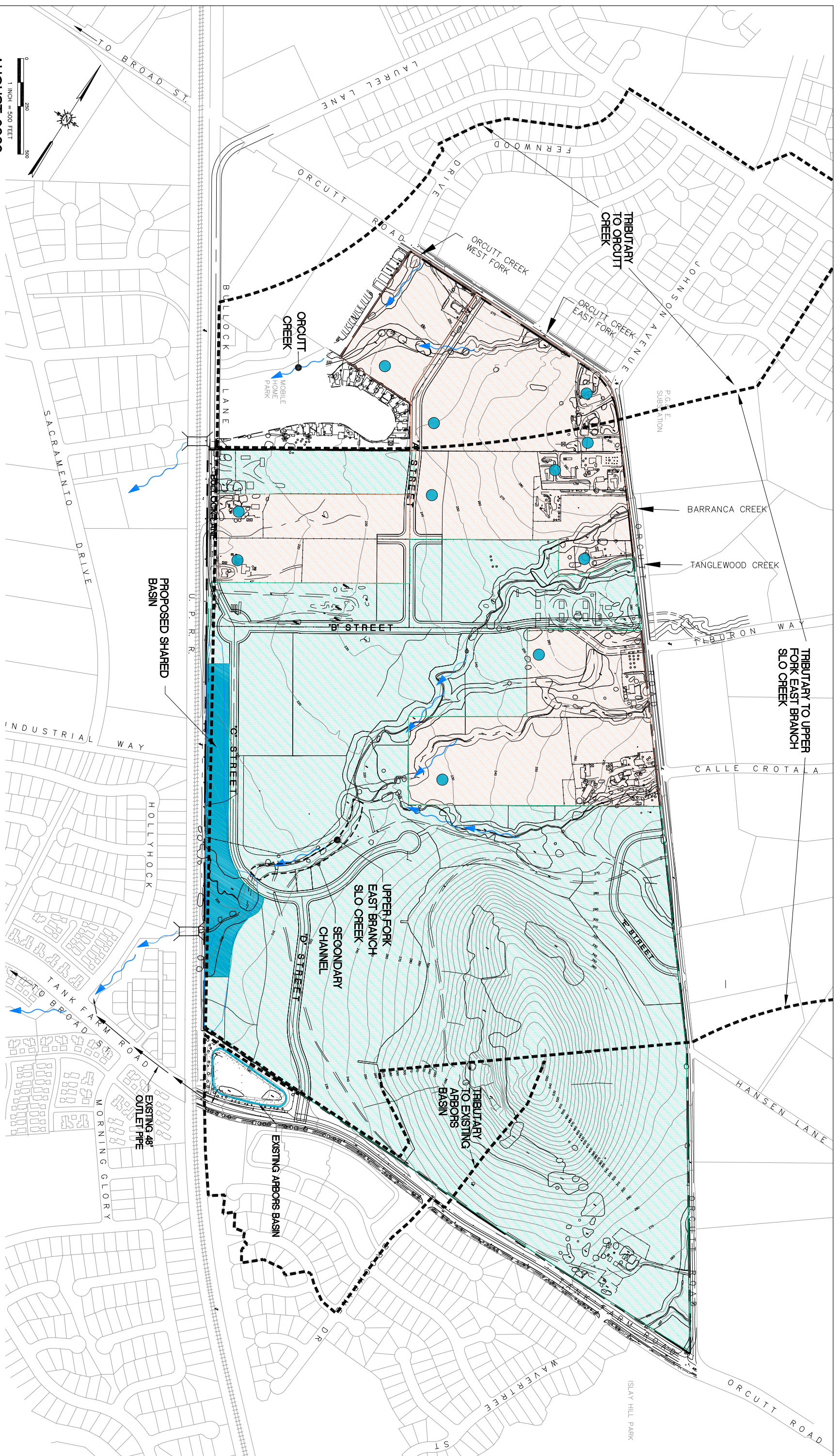
- Reduce 100-year post development peak runoff to 25-year pre-development rate.
- Reduce 50-year post development peak runoff to 20-year pre-development rate.
- Limit 10-year post development peak runoff to 10-year pre-development rate.
- Limit 2-year post development peak runoff to within 5 percent of the 2-year pre-development rate.

The DEIR analyzed the regional impact of the proposed basins designed to the above standards, and used analysis methodology consistent with the City's adopted WMP. The DEIR concluded that the detention basins provided sufficient mitigations such that there were no significant impacts to downstream receiving streams. Final design of the basins should utilize the City HEC-HMS hydrology model, as used in the hydrological study analysis.

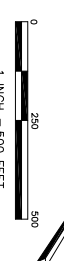
The proposed drainage plan includes a shared basin that provides detention for several subareas within the Specific Plan. Subareas not participating in the shared basin will be required to provide their own detention facilities. The shared basin is designed to incorporate concepts and strategies proposed on the UFEBSLO and will consist of a linked series of floodable terraces along the western boundary of the Orcutt Area covering approximately 7.0 acres. The floodable terrace system will have a capacity of approximately 30.0 acre-feet for detaining storm water. During design, the volume will be confirmed by detailed analysis based on the City HEC-HMS model and the criteria listed above. The shared basin is located at the low point of the Orcutt Area and is incorporated into the linear park system and the railroad buffer to provide a recreational amenity.

The shared basin may, as an alternative, be combined with the existing Arbors basin. The Arbors basin was originally designed with this in mind and has a 48-inch outlet pipe sized for this purpose. Combining the basins would offer advantages such as more efficient utilization of land, the flexibility and redundancy of two outlet pipes, and flexibility in phasing the project drainage system. Combining the basins would be subject to acceptance by the existing Arbors Homeowners' Association. Subareas within the Orcutt Creek watershed will require detention basins that release within the same watershed.

Smaller basins serving properties that are not participating in the shared basin will be located within the watersheds of Orcutt Creek and UFEBSLO. Figure 6.3 shows schematically the anticipated locations of these basins. The actual location of the basins may vary according to the detailed site design involved. The design of each area's basin shall adhere to the criteria above for detention and stormwater quality.



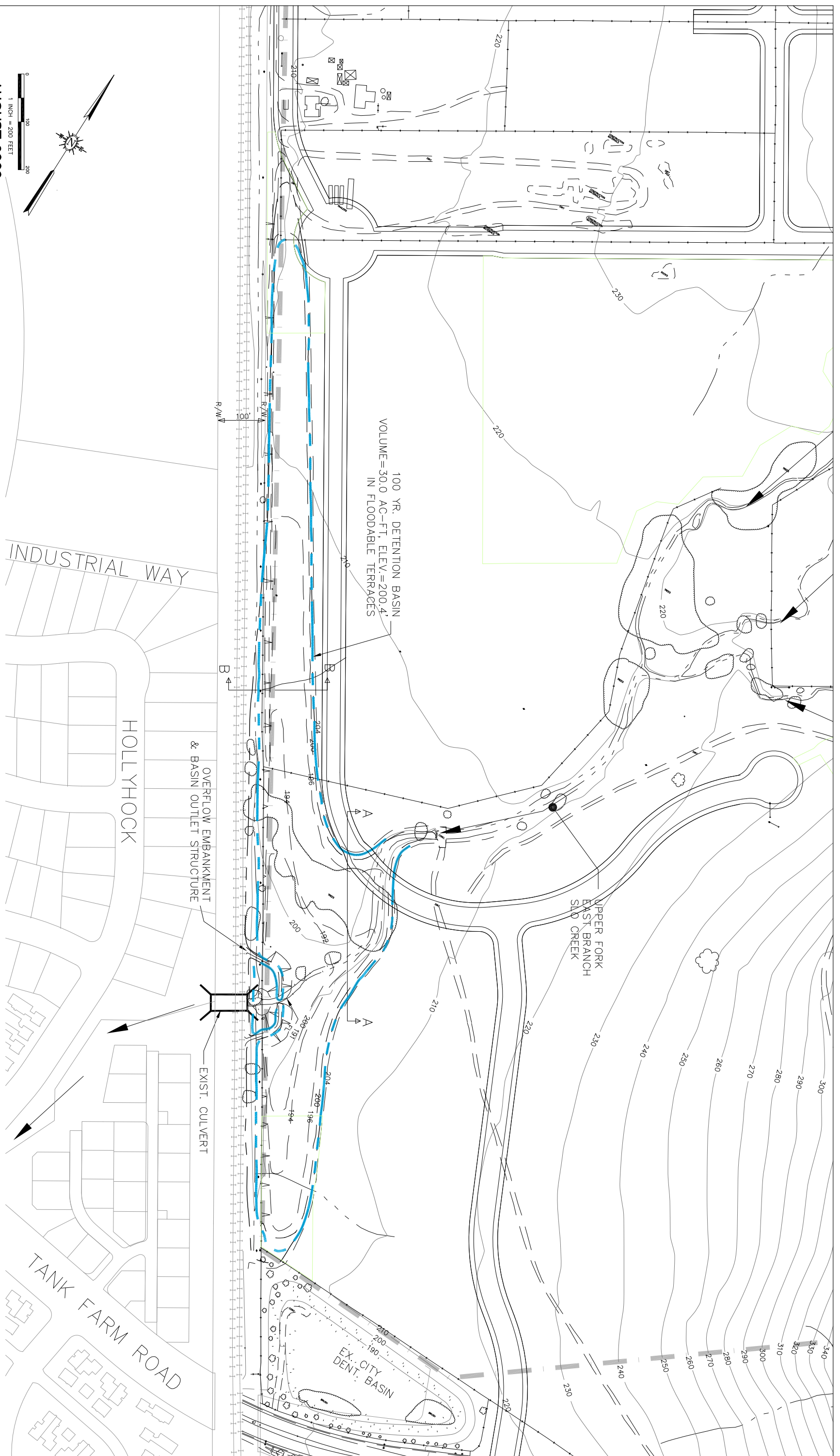
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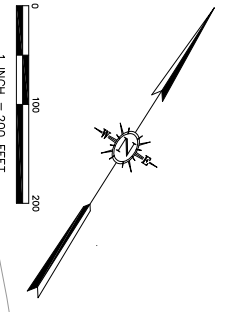
- LEGEND**
- LIMITS OF WATERSHED AREAS
 - - - ORCUTT AREA BOUNDARY
 - TRIBUTARY CHANNELS
 - EXISTING CULVERT
 - ORCUTT REGIONAL BASIN
 - EXISTING ARBORS BASIN
 - POTENTIAL NEW INDIVIDUAL BASIN LOCATION
 - PROPERTY SEPARATELY MITIGATING DRAINAGE
 - PROPERTY PARTICIPATING IN THE SHARED BASIN

ORCUTT EXPANSION AREA
S P E C I F I C P L A N

FIGURE 6.3
DRAINAGE PLAN



AUGUST 2009



LEGEND

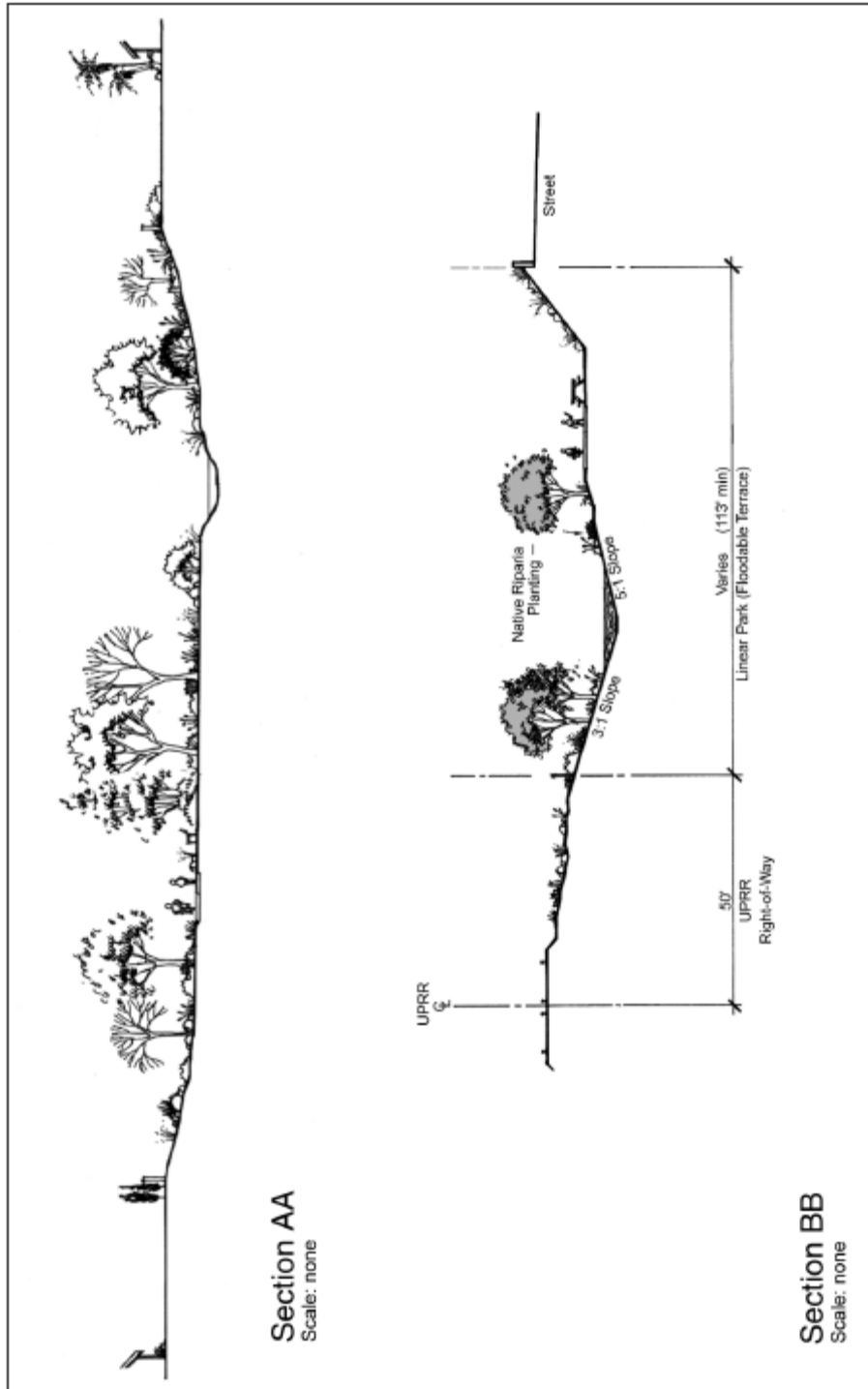
--- LIMITS OF ORCUTT REGIONAL BASIN

EXISTING CULVERT

ORCUTT EXPANSION AREA
S P E C I F I C P L A N

FIGURE 64
REGIONAL DETENTION BASIN PLAN

FIGURE 6.5 REGIONAL DETENTION BASIN CROSS-SECTIONS



6.4 ENERGY

Pacific Gas & Electric (PG&E) and The Gas Company will provide electricity and natural gas distribution. PG&E electrical service is currently available at the perimeter of the Orcutt Plan Area along Tank Farm and Orcutt Roads. New service will be placed in the right-of-way of “A”, “B”, “C”, and “D” Streets. All new electric lines will be placed underground. The Gas Company currently provides service to the Orcutt Area. Gas service lines currently exist at the perimeter of the site along Orcutt Road. Adequate volumes are available to serve the proposed uses in the Orcutt Plan Area. There are no existing gas mains located within the project property. It is anticipated that a new gas main will be constructed in one of the “A”, “B”, “C”, or “D” Streets, or Bullock Lane right-of-ways to serve the new residential development. (Refer to Chapter 4 Community Design for Energy Efficiency guidelines and standards in the Specific Plan area.)

6.5 TELECOMMUNICATIONS

SBC currently provides telephone service to the Orcutt Plan Area via overhead lines located along Orcutt Road. Cable television is provided by Charter Communications. These City-franchised private companies will extend their facilities into the Orcutt area as it develops. All new telecommunications lines within the Specific Plan area will be placed underground. The expanding range of broadcast, including satellite, services will be available for the Orcutt Area to the extent they are available throughout the San Luis Obispo area.

7 PUBLIC SERVICES

The Orcutt Plan Area and the surrounding area within the City limits are served by a number of public services including schools, fire and police protection, public transportation, solid waste disposal and recycling.

7.1 EDUCATION

Public education and secondary educational service are provided by the San Luis Coastal Unified School District. Public schools are supplemented by several private schools, with programs ranging from pre-school to high school levels. At completion, the Orcutt Area residential development is anticipated to have approximately 400 school-age children.

As discussed in the land use section of this plan (refer to Chapter 3), the Orcutt Area Specific Plan is designed to accommodate a new elementary school. All phases of planning and development of the site for a school will be the responsibility of the school district. Until a neighborhood elementary school is constructed or if no school is constructed in the Orcutt Area, elementary school children are expected to attend Sinshiemer School or Los Ranchos Elementary School. Older children in the Orcutt Area would attend Laguna Middle School and San Luis Obispo High School. The school district currently assesses school impact fees for residential development and commercial development. These fees are assessed when building permits are issued.

The Cuesta Community College District provides community college services. The Cuesta College campus, located west of San Luis Obispo, will be available to residents of the Orcutt Area. Cal Poly, on the north edge of the City, offers a wide range of regular and extension courses at the university level.

7.2 FIRE PROTECTION

The San Luis Obispo City Fire Department (SLOFD) will provide fire protection services in addition to paramedic services, hazardous materials mitigation, and disaster planning services. In case of a fire or medical emergency at the project site, Fire Station No. 3, located at the corner of Laurel Lane and Augusta Street, and Fire Station No. 1, located at the corner of Broad and Santa Barbara Streets, would provide the first response (**Figure 1.1**). Depending on the incident, Fire Stations No. 4 and 2 may also respond to provide back up.

The City's Safety Element of the General Plan designates the majority of the Orcutt Area as a Low Wildland Fire Hazard Area. The area along the base of Righetti Hill is designated a Moderate Hazard Area while the top of the Hill is designated a High Hazard Area.

Policy 7.2.1: Development will not occur in the High Hazard Area. Any development in the moderate hazard area will comply with the City's Safety Element Wildland Fire Safety Policies, which require non-combustible exteriors and defensible space. The Safety Element defines defensible space as "accessible space free of highly combustible vegetation and materials." (Safety Element Policy 3.1).

Policy 7.2.2: Development of the plan area will conform to the most recent Uniform Fire Code and City fire protection standards.

Program 7.2.2a: Road widths and internal circulation, as well as the placement of fire hydrants, shall be designed with the guidance of the Fire Department. All primary internal access ways will allow unhindered Fire Department access and maneuvering during emergencies and shall be designed as fire lanes with adequate width, vertical clearance, grades, and turning radius.

ORCUTT AREA SPECIFIC PLAN

7 Public Services

- Program 7.2.2b: New development will have adequate water volumes and fire hydrants per the City's requirements for domestic and fire protection water needs. Public fire hydrants will be installed per the SLOFD development guidelines.
- Program 7.2.2c: All buildings will be provided with an approved, automatic fire-sprinkler system per SLOFD requirements.

7.3 LAW ENFORCEMENT

The San Luis Obispo Police Department will provide law enforcement services for the Orcutt Area. The police station is located at 1042 Walnut Street (corner of Walnut and Santa Rosa Streets) in downtown San Luis Obispo (**Figure 1.1**). Average citywide emergency response time varies, depending on the location of the patrol cars when they are contacted. However, since the Orcutt Area is located within 3 to 4 miles of the Santa Rosa Street Station, response time to the site is expected to be less than five minutes. In addition, the County has law enforcement services available at the San Luis Obispo County Airport that could provide mutual aid response in an emergency, if needed. Offices of the County district attorney and municipal and superior courts are located in downtown San Luis Obispo.

7.4 HEALTH

The County provides environmental health services and some animal control services from offices located in the San Luis Obispo City area. There are two private hospitals and several private clinics in the city. No special provisions are anticipated to meet the service demands resulting from development of the Orcutt Area.

7.5 PUBLIC AREAS MAINTENANCE

The City will maintain public facilities in the Orcutt Plan Area including parks, open space, and creek corridors; public streets; bicycle lanes and paths; and pedestrian paths. Street maintenance includes roadway pavement and markings, curb, gutter and sidewalk, and signs, street lighting, and signals. Adjacent properties will maintain the curb strip (planted area between the curb and the sidewalk) except for trees in the curb strip, which are maintained by the City. Maintenance of planting strips and other streetscaping along "A", "B", "C", and "D" Streets, Bullock Lane Orcutt Road, and Tank Farm Road will be the responsibility of a Master Homeowners Association. A master homeowners association or homeowners associations by individual development will also be responsible for maintenance of detention basins in the Orcutt Area.

Privately owned but shared outdoor areas, such as condominium common areas, will be maintained by homeowner's associations. The owners of the multi-family apartments will maintain privately owned common outdoor areas such as children's playgrounds and outdoor greens.

7.6 TRANSPORTATION

Transportation includes public streets, bicycle paths, transit services, trains, and air service. The City will maintain public streets and bicycle and pedestrian paths. The City expects to continue to provide citywide bus service through a contract operator and intends to extend service into the Orcutt Area as street connections and development are completed. Potential bus routes and stops in the Orcutt Area are shown on **Figure 5.1**; however, final routes, stops, and schedules are not included in the Specific Plan and will be determined in consultation with the transit authority. Regional transit agencies are expected to continue providing connections between San Luis Obispo and other points in the county. Existing transportation services provided by Amtrak, air carriers and bus charter companies are expected to continue.

7.7 SOLID WASTE AND RECYCLING

Solid waste generated by the new development will be delivered to Cold Canyon Landfill. Based on the current rate of disposal, this facility has a capacity to accept solid waste for at least 20 years. San Luis Garbage will provide solid waste, recycling and green waste pick-up service to the area.

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8 PUBLIC FACILITIES FINANCING

8.1 INTRODUCTION

Chapter 8 analyzes the public facilities burden that must be carried by the land uses proposed in the Orcutt Area Specific Plan (“OASP” or “Project”) and presents a “pay-as-you-go” financing strategy to fund that burden. The burden consists of infrastructure and related costs necessary to serve the Project directly, plus development impact fees that would be imposed on the Project for other City-wide capital improvements that the Project must contribute to. “Pay-as-you-go” refers to the fact that the phasing of public improvements has been designed to insure that funds will be available to cover the cost of facilities when they are required. The facilities covered by this financing plan do not include in-tract improvements for subdivisions within the OASP; it is expected that these costs will be privately funded by the OASP developers.

In summary, Chapter 8 includes the following information:

- A description of public facilities required to serve future development in the OASP area.
- An overview of the costs of required public facilities, including an allocation of the costs to the proposed future development.
- Identification of the total one-time burdens (impact fees) to be collected within the Project area to fund facilities on a pay-as-you-go basis .
- A summary of implementation measures that must be enacted by the City.
- A separate report, titled Orcutt Area Specific Plan, Public Facilities Financing Plan (PFFP), also includes detailed cost estimates of each public facility. These cost estimates are the basis for the facility costs used to develop this fee program and are kept on file in the Community Development Department and updated from time to time as discussed in Section 8.5.1 of this chapter.

The phasing plan for public facilities described later in Section 8.4.2 of this chapter was developed to accomplish two key goals. First, to insure that the public facilities needed to serve Orcutt Area development are constructed as soon as possible for the benefit of future Orcutt Area residents. And, second, to insure that facilities are phased in as fees become available to pay for their construction, which will reduce financing costs and help lower the cost of constructing new housing in the Orcutt Area.

8.2 FACILITY NEEDS AND COST ESTIMATES

The Orcutt Area Specific Plan describes in detail the water, wastewater, storm drainage, roads, parks, and miscellaneous improvements proposed to meet the needs of the community. The City currently has a development impact fee program in place that will fund the water and wastewater improvements and some of the road improvements for the Project. For storm drainage, individual land owners may utilize individual onsite detention basins, which are considered in-tract improvements, or may participate in a combined detention basin with other owners. Owners requesting participation in a combined basin will

enter into a separate cost sharing agreement with the other participating owners. The remaining Project-specific costs for roads, bridges, pedestrian and bicycle paths, and parks and recreation that are not covered by existing development impact fees or other funding mechanisms are the focus of Chapter 8. The total cost for the improvements required for the OASP is estimated to be approximately \$14 million, as summarized in Table 8.1. Table 8.2, on the following page, lists all of the projects proposed under each category, including the individual project cost estimates and OASP fair share percentage.

TABLE 8.1 – INFRASTRUCTURE COST ESTIMATE SUMMARY

Improvement	Total Cost
Transportation	\$4,237,034
Pedestrian and Bicycle Paths	\$1,775,755
Parks and Recreation	\$4,448,000
Parkland	\$3,678,000
Total	\$14,138,788

8.2.1 Transportation Improvements and Pedestrian and Bicycle Paths

The circulation plan for the Project provides direct connections between the existing arterials (Orcutt Road and Tank Farm Road) and the new roads within the OASP. Bicycle and pedestrian circulation routes will provide access throughout the interior of the Project and will connect to the existing pedestrian and bicycle network outside the Orcutt area. The following is a list of circulation system projects required to serve the OASP:

- Orcutt Road / Tank Farm Road improvements
- Broad Street / South Street – Santa Barbara Road improvements
- Broad Street / Tank Farm Road second southbound left turn lane and a second northbound left turn lane
- Orcutt Road / Johnson Avenue improvements
- Broad Street / Prado Road extension and second northbound left turn lane
- Orcutt Road widening – Broad to Laurel
- Bullock Lane realignment
- Relocation of Hanson Road or reducing the grade on Orcutt at Hanson Road
- Transit stops (5)
- Orcutt expansion area bridges – A, B, and C
- Pedestrian and bicycle paths
- Pedestrian / bike overpass
- Bike path extension over Tank Farm Road

The total cost for street and bridge improvements is estimated to be approximately \$6.6 million and the Project's fair share of these improvements is estimated to be \$4.2 million. An additional \$1.77 million is anticipated for the OASP's fair share of pedestrian and bicycle paths, which are estimated to cost a total of \$2.9 million. Costs include site preparation, earthwork, hardscape, traffic markings, and miscellaneous improvements.

TABLE 8.2 – DETAILED INFRASTRUCTURE COST ESTIMATES

<u>Item</u>	<u>Gross Total Cost</u>	<u>OASP Fair Share Percentage</u>	<u>Net Total Cost</u>
Transportation			
<i>Street Improvements</i>			
Orcutt Road/Tank Farm Road	\$927,978	100.0%	\$927,978
Broad Street/South St-Santa Barbara Road	\$1,500,000	25.4%	\$381,000
Broad Street/Tank Farm Rd	\$444,808	50.0%	\$222,404
Orcutt Road/Johnson Ave	\$300,004	100.0%	\$300,004
Broad Street/Prado Road Extension Second Northbound Left Turn Lane	\$135,905	100.0%	\$135,905
Orcutt Road Widening	\$345,590 ¹	89.9%	\$310,685
Bullock Lane Realignment	\$355,796	70.0%	\$249,057
Relocating Hanson Rd or Reducing the Grade on Orcutt at Hanson Rd	\$50,000	100.0%	\$50,000
Transit Stops	\$50,000	100.0%	\$50,000
Subtotal Street Improvements	<u>\$4,110,081</u>		<u>\$2,627,034</u>
<i>Orcutt Expansion Area Bridges</i>			
Bridge A	\$680,000	100.0%	\$680,000
Bridge B	\$420,000	100.0%	\$420,000
Bridge C	\$510,000	100.0%	\$510,000
Subtotal Orcutt Expansion Area Bridges	<u>\$1,610,000</u>		<u>\$1,610,000</u>
Total Transportation	\$5,720,081		\$4,237,034
Pedestrian and Bicycle Paths			
Pedestrian and Bicycle Paths	\$648,200	100.0%	\$648,200
Pedestrian/Bike Overpass	\$880,000 ²	100.0%	\$880,000
Bike Path Extension Over Tank Farm Road	\$495,109	50.0%	\$247,555
Total Pedestrian and Bicycle Paths	\$2,023,309		\$1,775,755
Parks & Recreation			
Central Neighborhood Park - Main Portion South of Creeks	\$3,628,000	100.0%	\$3,628,000
Central Neighborhood Park - Phase 2 Portions North of Creeks	\$500,000	100.0%	\$500,000
Pocket Park	\$220,000	100.0%	\$220,000
Linear Park System	\$100,000	100.0%	\$100,000
Total Parks & Recreation	\$4,448,000		\$4,448,000
Parkland	\$3,678,000	100.0%	\$3,678,000
Total Project-Specific Infrastructure Costs	\$15,869,390		\$14,138,788

¹ Represents the Gross Total Cost attributable to development. The total cost of the project is \$1,250,000.

² Represents the Gross Total Cost attributable to development. The total cost of the project is \$1,760,000.

Sources: The Wallace Group; City of San Luis Obispo; Goodwin Consulting Group, Inc.

6/9/2010

8.2.2 Parks and Recreation, Land and Improvements

The Orcutt Area Specific Plan provides for approximately 16.3 acres of improved parkland. In addition, the San Luis Coastal Unified School District is expected to develop an elementary school within the Orcutt Area, or nearby, to serve future residents. It is normal for the City to enter into Joint-Use Agreements with the School District, which would provide additional parkland benefits to City residents. The amount of parkland listed below, plus future recreation facilities that would be developed with a new elementary school, fully satisfy parkland requirements for the Orcutt Area.

A proposed 12-acre neighborhood park located at the center of the Project will serve as a community gathering place for casual recreation and sporting events by providing a variety of active recreation facilities. In addition, a linear park is proposed that will serve a dual purpose as both an area-wide detention basin and a recreation area, and a smaller pocket park is planned within the low and medium density residential neighborhoods. A 2.5 acre “trail junction” park will provide passive parkland adjacent to trailheads at the base of Righetti Hill. The following list summarizes the parks and recreation projects planned to serve the Project:

- Central Neighborhood Park – 11.13 Acres
- Garay Portion of Neighborhood Park (if Garay property is developed) - 0.87 Acres
- Pocket Park – 0.26 Acre
- Linear Park System – 1.54 Acres
- Trail Junction Park – 2.50 Acres

The total cost of park and recreation improvements to be funded by the Project is estimated to be approximately \$4.45 million. The parks will be improved with a wide range of features, including tennis courts, a soccer field, perimeter paths, creek enhancements, public art, restrooms, and parking as described in Chapter 2 of the OASP.

The total land cost will be \$3,678,000, or \$300,000 per acre for the Neighborhood Park and the Pocket Park (12.26 acres total). Land for the Linear Park System and the Trail Junction Park, which have no development potential (4.04 acres total), will be dedicated by the property owner for parkland purposes at no cost.

8.3 OASP FEE PROGRAM AND OTHER FEES

Development impact fees are monetary exactions (as opposed to taxes or special assessments) that are charged by local agencies in conjunction with approval of a development project. The fees are paid by builders or developers, typically at the time a building permit is issued. Impact fees are levied for the purpose of defraying all or a portion of the costs of a public facility, improvement, or amenity that benefits the project. The collection of impact fees does not require formation of a special district; an impact fee program is implemented by a public agency’s adoption of a resolution or ordinance.

Assembly Bill 1600 (herein “AB 1600”), which was enacted by the State of California in 1987, created Section 66000 et seq. of the Government Code. In order to establish, increase, or impose a fee as a condition of approval of a development project, AB 1600 (also known as the Mitigation Fee Act) requires a public agency to specifically identify the type of - and need for - public facilities funded by the impact fees. The public agency must also determine the reasonable relationship, or “nexus,” between the development project and the cost of the facilities, thereby explaining the need to impose a fee.

Impact fees will be an important component of paying for public improvements in the Orcutt Area. Once the Project area is annexed into the City, a fee ordinance must be adopted before development can occur; the fee program may also be updated and revised as part of future development phases.

8.3.1 Proposed Orcutt Area Specific Plan Fee Program

The primary source of funding for Project-specific improvements will be the proposed Orcutt Area Specific Plan Fee Program. In order to fairly allocate facility costs funded by the fee program among land uses within the OASP, it is necessary to use factors that relate the amount of benefit a land use will derive from a given capital facility relative to that of other land uses. Table 8.2 outlines the appropriate benefit unit classification used for each facility, and summarizes the resulting cost per unit. The individual factors for transportation and pedestrian and bicycle path improvements are consistent with those documented in the OASP transportation impact analysis, included in the Final Program Environmental Impact Report for the project. Park and recreation improvements are allocated to the residential land uses based on the number of residents served.

TABLE 8.3 – BENEFIT UNIT CLASSIFICATIONS AND COST PER UNIT

Capital Facility:	Transportation	Pedestrian and Bicycle Paths	Parks & Recreation	Parkland	Total Cost Allocation	Total Facility Costs
Benefit Unit:	Daily Trip Rate	Daily Trip Rate	Residents Served	Residents Served		
Capital Costs:	\$4,237,034	\$1,775,755	\$4,448,000	\$3,678,000		\$14,138,788
	<i>Cost per Unit</i>				<i>per Unit</i>	
Single Family	\$5,217	\$2,186	\$5,352	\$4,425	\$17,181	\$8,985,402
Multi-Family	\$3,644	\$1,527	\$3,983	\$3,293	\$12,448	\$5,153,386
Total						\$14,138,788

Source: Goodwin Consulting Group, Inc.

6/9/2010

As previously mentioned, the public facilities identified in this analysis were designed and sized to serve the residential development in the OASP. The proposed commercial uses are a minor part of the total Project, representing less than one-half of 1% of the net developable acreage. Therefore, the cost of public facilities is not allocated to these commercial land uses because they will be developed only as a result of the demand created by the residential development. As discussed in Chapter 3, this fee exemption is intended to serve as an incentive to help achieve Goal 3.2c, which envisions a commercial area that primarily serves the needs of those living, working or using the neighborhood park in the Orcutt Area. (Also see Program 3.2.19.c).

8.3.2 Existing Development Impact Fee Programs

Development in the OASP is expected to participate in the existing City-wide development impact fee programs for transportation, water, and sewer facilities. These fees are in addition to the Project-specific fees discussed above and will fund the Project’s fair share of City-wide public facility costs. It should be noted that there are no duplicated transportation infrastructure costs between the Project-specific transportation fees and the City-wide fees. The City-wide development impact fees for each land use category are shown in Table 8.4.

TABLE 8.4 – CITY-WIDE IMPACT FEES BY DEVELOPMENT TYPE

Development Type	Water Impact Fee	Wastewater Impact Fee	Transportation Impact Fee	Total
Single-Family	\$15,919	\$6,946	\$3,220	\$26,085
Multi-Family	\$12,735	\$5,557	\$2,858	\$21,150

8.3.3 Specific Plan and EIR Fee

New development in the Orcutt Area Specific Plan will also be subject to a fee that will be used to reimburse the City and certain land owners for EIR preparation costs. In addition, the City will be reimbursed for its costs associated with preparation of the specific plan. The total cost of \$500,000 is spread equally to the residential land uses on a per-acre basis, resulting in a fee of \$4,413 per acre. This amount is translated into a fee per residential unit based on the density assumptions for each property, and summarized in Table 8.5.

TABLE 8.5 – SPECIFIC PLAN AND EIR FEE

Development Type	Specific Plan and EIR Fee
Single-Family	\$737
Multi-Family	\$276

8.3.4 Summary of Impact Fees

The Project-specific, City-wide, and other fees are presented in Table 8.6, and are added together to show the total gross burden by land use on a per-unit and per-acre basis. A portion of the OASP has the potential to develop as a school instead of residential. The fees for residential are shown both per unit and per acre to (i) ensure that the amount of expected fee revenue will be maintained and (ii) to document the school’s infrastructure obligation, or burden, so that it can be considered in a possible land sale transaction between the school district and the land owner.

TABLE 8.6 – TOTAL PROJECT SPECIFIC PLUS CITY-WIDE AND OTHER IMPACT FEES

Land Use	Project-Specific Impact Fees					City-Wide Impact Fees				Other Impact Fees			Total Gross Fees per Net Acre
	Trans- portation	Pedestrian and Bicycle Paths	Parks & Recreation	Parkland	Total Project- Specific Impact Fees	Trans- portation Impact Fee	Water Impact Fee	Sewer Impact Fee	Total City-Wide Fees	Specific Plan and EIR Fee	Total Other Fees	Total Gross Fees per Unit	
Single Family	\$5,217	\$2,186	\$5,352	\$4,425	\$17,181	\$3,220	\$15,919	\$6,946	\$26,085	\$737	\$737	\$44,003	\$263,372
Multi-Family	\$3,644	\$1,527	\$3,983	\$3,293	\$12,448	\$2,858	\$12,735	\$5,557	\$21,150	\$276	\$276	\$33,874	\$541,044

8.4 FACILITY PHASING AND PROJECT CASH FLOW

8.4.1 Absorption Assumptions

Development in the OASP is anticipated to occur in three phases. Based on information provided in the Specific Plan and input from the City, an estimated absorption schedule that is also consistent with the City’s Growth Management Phasing Schedule for the Orcutt area was created. Phase 1 of the Project is assumed to have a five-year absorption horizon, and both Phases 2 and 3 are assumed to develop over the subsequent three-year time periods.

While actual absorption rates will vary, these projections provide a useful schedule to estimate the timing of fee revenue and facility phasing. The annual and cumulative absorption assumptions are presented in Tables 8.7 and 8.8.

TABLE 8.7 – ANNUAL ABSORPTION ASSUMPTIONS

Land Use	Total	Phase 1					Phase 2			Phase 3		
		2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Single Family	523	55	55	41	42	42	77	53	51	37	36	34
Multi-Family	414	30	30	25	25	25	52	64	63	63	37	0
Total	937	85	85	66	67	67	129	117	114	100	73	34

Sources: Orcutt Area Specific Plan (Draft, June 2006); City of San Luis Obispo Growth Management Phasing Schedule (General Plan Annual Report, 2006); City of San Luis Obispo; Goodwin Consulting Group, Inc. 6/9/2010

TABLE 8.8 – CUMULATIVE ABSORPTION ASSUMPTIONS

Land Use	Total	Phase 1					Phase 2			Phase 3		
		2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Single Family	523	55	110	151	193	235	312	365	416	453	489	523
Multi-Family	414	30	60	85	110	135	187	251	314	377	414	414
Total	937	85	170	236	303	370	499	616	730	830	903	937

Sources: Orcutt Area Specific Plan (Draft, June 2006); City of San Luis Obispo Growth Management Phasing Schedule (General Plan Annual Report, 2006); City of San Luis Obispo; Goodwin Consulting Group, Inc. 6/9/2010

8.4.2 Facility Phasing

Facilities will be required at various stages of Project development. The majority of the facility phasing is linked to residential development; for example, the Orcutt Road/Tank Farm Road transportation improvements are required to be completed prior to the development of the 371st unit. A limited number

of facilities will need to be installed during the first years of development, including construction of the Orcutt expansion area Bridge A, and two transit stops. Other facilities, such as the Broad Street/Prado Road intersection improvements, will be constructed as impact fee revenues become available, which is not expected to occur until the last few years of Project development.

A conscious effort was made during the design of these phasing assumptions to ensure funding would be available for these improvements on a pay-as-you-go basis. As noted in the cash flow section below, no cumulative funding gaps are projected as a result of these phasing assumptions; therefore, lump-sum financing with bonds or other mechanisms can be avoided.

Table 8.9 delineates the phasing assumptions for the OASP facility costs and Table 8.10 presents the annual costs based on those assumptions.

TABLE 8.9 – OASP FACILITY PHASING ASSUMPTIONS

Cost Category	Cost Phasing Assumptions
Transportation	
Orcutt Road/Tank Farm Road	Prior to 371 Units
Broad Street/South St-Santa Barbara Rd.	Prior to 304 Units
Broad Street/Tank Farm Rd	Prior to 304 Units
Orcutt Road/Johnson Ave	Prior to 617 Units
Broad Street/Prado Road Extension	As Fee Revenue Becomes Available ¹
Orcutt Road Widening	As Fee Revenue Becomes Available (Reimburse to Orcutt Associates, LLC) ¹
Bullock Lane Realignment	Prior to 500 Units
Relocating Hanson Rd	Prior to 731 Units
Transit Stops	One Stop per Year, Beginning in Year Three
Bridge A	100% in Year Two
Bridge B	100% in Year Six
Bridge C	100% in Year Three
Pedestrian and Bicycle Paths	
Pedestrian and Bicycle Paths	As Fee Revenue Becomes Available ¹
Pedestrian/Bike Overpass	Prior to 831 Units
Bike Path Extension Over Tank Farm Rd.	Prior to 500 Units
Parks & Recreation/Parkland	
	75% Spread Evenly Over Five Years, Beginning in Year Four; 25% Spread Evenly Over Two Years, Beginning in Year Nine

¹ These infrastructure items and reimbursements are anticipated to occur in the three years of Phase 3 (see Table 14), in which the costs do not cause a cumulative net deficit to occur (see Table 15).

Sources: City of San Luis Obispo; Goodwin Consulting Group, Inc.

6/9/2010

*ORCUTT AREA SPECIFIC PLAN
08 Public Facilities Financing*

TABLE 8.10 – OASP ANNUAL FACILITY COST PHASING

Item	Total	Phase 1					Phase 2			Phase 3		
		2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Transportation												
Orcutt Road/Tank Farm Road	\$927,978	\$0	\$0	\$0	\$0	\$927,978	\$0	\$0	\$0	\$0	\$0	\$0
Broad Street/South St-Santa Barbara Rd.	\$381,000	\$0	\$0	\$0	\$381,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Broad Street/Tank Farm Rd	\$222,404	\$0	\$0	\$0	\$222,404	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Orcutt Road/Johnson Ave	\$300,004	\$0	\$0	\$0	\$0	\$0	\$0	\$300,004	\$0	\$0	\$0	\$0
Broad Street/Prado Road Extension	\$135,905	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$101,929	\$33,976
Orcutt Road Widening	\$310,685	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$310,685	\$0
Bullock Lane Realignment	\$249,057	\$0	\$0	\$0	\$0	\$0	\$249,057	\$0	\$0	\$0	\$0	\$0
Relocating Hanson Rd	\$50,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$50,000	\$0	\$0	\$0
Transit Stops	\$50,000	\$0	\$0	\$10,000	\$10,000	\$10,000	\$10,000	\$10,000	\$0	\$0	\$0	\$0
Bridge A	\$680,000	\$0	\$680,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Bridge B	\$420,000	\$0	\$0	\$0	\$0	\$0	\$420,000	\$0	\$0	\$0	\$0	\$0
Bridge C	\$510,000	\$0	\$0	\$510,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Subtotal Transportation Improvements	\$4,237,034	\$0	\$680,000	\$520,000	\$613,404	\$937,978	\$679,057	\$310,004	\$50,000	\$0	\$412,614	\$33,976
Pedestrian and Bicycle Paths												
Pedestrian and Bicycle Paths	\$648,200	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$648,200
Pedestrian/Bike Overpass	\$880,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$880,000	\$0	\$0
Bike Path Extension Over Tank Farm Rd.	\$247,555	\$0	\$0	\$0	\$0	\$0	\$247,555	\$0	\$0	\$0	\$0	\$0
Subtotal Pedestrian and Bicycle Paths	\$1,775,755	\$0	\$0	\$0	\$0	\$0	\$247,555	\$0	\$0	\$880,000	\$0	\$648,200
Parks & Recreation/Parkland												
	\$8,126,000	\$0	\$0	\$0	\$1,218,900	\$1,218,900	\$1,218,900	\$1,218,900	\$1,218,900	\$1,015,750	\$1,015,750	\$0
Total Infrastructure Costs	\$14,138,788	\$0	\$680,000	\$520,000	\$1,832,304	\$2,156,878	\$2,145,512	\$1,528,904	\$1,268,900	\$1,895,750	\$1,428,364	\$682,176

Sources: City of San Luis Obispo; Goodwin Consulting Group, Inc.

6/9/2010

TABLE 8.11 – OASP PROJECT SPECIFIC REVENUE VS. COSTS

Improvement	Total	Phase 1					Phase 2			Phase 3		
		2009	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Transportation												
Revenues	\$4,237,034	\$396,244	\$396,244	\$304,990	\$310,206	\$310,206	\$591,184	\$509,714	\$495,637	\$422,603	\$322,637	\$177,367
Costs	(\$4,237,034)	\$0	(\$680,000)	(\$520,000)	(\$613,404)	(\$937,978)	(\$679,057)	(\$310,004)	(\$50,000)	\$0	(\$412,614)	(\$33,976)
Net	\$0	\$396,244	(\$283,756)	(\$215,010)	(\$303,198)	(\$627,772)	(\$87,873)	\$199,710	\$445,637	\$422,603	(\$89,977)	\$143,391
Cumulative Net		\$396,244	\$112,489	(\$102,522)	(\$405,720)	(\$1,033,491)	(\$1,121,364)	(\$921,654)	(\$476,017)	(\$53,414)	(\$143,391)	\$0
Pedestrian and Bicycle Paths												
Revenues	\$1,775,755	\$166,067	\$166,067	\$127,822	\$130,008	\$130,008	\$247,767	\$213,623	\$207,723	\$177,114	\$135,218	\$74,335
Costs	(\$1,775,755)	\$0	\$0	\$0	\$0	\$0	(\$247,555)	\$0	\$0	(\$880,000)	\$0	(\$648,200)
Net	\$0	\$166,067	\$166,067	\$127,822	\$130,008	\$130,008	\$213	\$213,623	\$207,723	(\$702,886)	\$135,218	(\$573,865)
Cumulative Net		\$166,067	\$332,135	\$459,957	\$589,965	\$719,974	\$720,186	\$933,809	\$1,141,532	\$438,647	\$573,865	\$0
Parks & Recreation/Parkland												
Revenues	\$8,126,000	\$756,050	\$756,050	\$582,784	\$592,561	\$592,561	\$1,131,232	\$983,888	\$957,057	\$820,172	\$621,211	\$332,435
Costs	(\$8,126,000)	\$0	\$0	\$0	(\$1,218,900)	(\$1,218,900)	(\$1,218,900)	(\$1,218,900)	(\$1,218,900)	(\$1,015,750)	(\$1,015,750)	\$0
Net	\$0	\$756,050	\$756,050	\$582,784	(\$626,339)	(\$626,339)	(\$87,668)	(\$235,012)	(\$261,843)	(\$195,578)	(\$394,539)	\$332,435
Cumulative Net		\$756,050	\$1,512,100	\$2,094,883	\$1,468,544	\$842,205	\$754,538	\$519,526	\$257,682	\$62,104	(\$332,435)	\$0
Total												
Revenues	\$14,138,788	\$1,318,361	\$1,318,361	\$1,015,595	\$1,032,776	\$1,032,776	\$1,970,184	\$1,707,225	\$1,660,416	\$1,419,889	\$1,079,066	\$584,137
Costs	(\$14,138,788)	\$0	(\$680,000)	(\$520,000)	(\$1,832,304)	(\$2,156,878)	(\$2,145,512)	(\$1,528,904)	(\$1,268,900)	(\$1,895,750)	(\$1,428,364)	(\$682,176)
Net	\$0	\$1,318,361	\$638,361	\$495,595	(\$799,528)	(\$1,124,102)	(\$175,328)	\$178,321	\$391,516	(\$475,861)	(\$349,298)	(\$98,039)
Cumulative Net		\$1,318,361	\$1,956,723	\$2,452,318	\$1,652,790	\$528,688	\$353,360	\$531,681	\$923,197	\$447,337	\$98,039	\$0

Source: Goodwin Consulting Group, Inc.

6/9/2010

8.4.3 Project Cash Flow

Table 8.11 compares the phasing of facility costs to the timing of fee revenues for each cost category and in total. Based on the absorption and phasing assumptions discussed above, there will be positive cash flows in some years and negative cash flows in others; however, it is estimated that there will be a total cumulative surplus in each year except for two, in which the revenues and costs will break even.

8.5 IMPLEMENTATION PLAN

The Orcutt Area Specific Plan identifies a program for significant residential growth, and limited non-residential growth, within the City of San Luis Obispo and will be subject to updates and revisions in future years as development applications are submitted and processed.

This financing strategy for public improvements is based on assumptions of land use, facility demands, facility standards and design, and cost estimates. Each of these assumptions may be subject to change in future years; therefore, Chapter 8 may also be revised to reflect these changes. The ongoing implementation of the OASP fee program will be parallel to the continued monitoring of the land use assumptions made in the Specific Plan, and will require the same degree of time and effort to keep it current and useful. In this manner, Chapter 8 will guide the overall funding of community infrastructure required to serve the Specific Plan. Following is a summary of many of the tasks associated with implementation of Chapter 8, Public Facilities Financing.

8.5.1 Updates and Revisions

As noted above, changes are likely to occur in facility plans, land use plans, or cost estimates. When these items are revised, there will be a corresponding change in the fair share cost allocation to each land use in the OASP. Land use and facility changes will result in revisions to the benefit analysis and corresponding cost allocation to each land use. To the extent some projects in the OASP will have been developed and will have paid their fair share as defined at the time they were built, revisions will apply only to future new development.

As the City will adopt new ordinances and update existing ordinances on an ongoing basis, fees will be adjusted based on actual costs realized after construction bids have been received for public facilities. If actual costs are higher than expected, again, the City will have to increase fees and/or rely on the terms of an acquisition agreement to avoid a financing deficit in future years. In the same manner, when additional funding sources for specific projects (such as grant funding for transportation projects) becomes available, costs allocated to future development under the OASP fee program will be reduced.

8.5.2 Action Items

Prior to commencement of development in the Specific Plan, the City will need to adopt a fee ordinance or resolution implementing the OASP fee program for each type of capital facility. The initial ordinance will reflect fees based on information available at that time. Fees will be adjusted annually or on a more frequent basis to reflect actual costs and current cost estimates.

Pursuant to Section 66006 of the Government Code, the City will establish a separate OASP capital facility account and a unique fund for each type of public facility for which fees are collected. Establishment of this account will prevent commingling of the OASP fees with other City revenues and funds. Interest income earned by fee revenues in this account will be deposited in the account and applied to facility construction costs. Within one hundred eighty (180) days of the close of each fiscal year, the City will make information pertaining to the account [as required by Section 66006 (b)(1)] available to the public and will review this information at a regularly scheduled public hearing.

In order to maximize the efficiency of the capital improvements program, the City may borrow money from one fund within the OASP account to pay for facilities financed by another fund within the account. This borrowing will occur when one type of facility is needed immediately, while another type is not needed for a number of years. The City will monitor such borrowing on an ongoing basis and will repay funds from which fee revenues were borrowed in a timely manner and in an amount equal to the original amount borrowed plus the interest that would have accrued had the money not been borrowed from the fund.

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9 IMPLEMENTATION

9.1 ENVIRONMENTAL REVIEW

The OASP addresses the land uses, types of development, and densities proposed for the 230.85-acre Orcutt Area. It provides a detailed description of the project that was evaluated in the Final Program Environmental Impact Report for the Orcutt Area Specific Plan (FEIR).

The FEIR identifies the environmental impacts associated with implementation of the land use program described in this specific plan. Most of the mitigation measures have been incorporated directly into the OASP, however, Appendix C lists additional mitigation measures that are not found in the body of the plan. These additional mitigation measures must be reviewed and implemented as part of future applications to the City of San Luis Obispo to develop property under the OASP. These mitigation measures include requirements for timed, seasonal botanical studies, cultural resource investigations and other work identified in the FEIR as necessary to address the potential impacts associated with development.

When new subdivisions are proposed under the OASP, City staff will require applicants to submit an application for an Initial Study of Environmental Impact, in compliance with California Environmental Quality Act guidelines. Future environmental studies will use the FEIR as a reference and a new EIR will only be required when new impacts are discovered that are deemed to be significant and unavoidable.

9.2 ANNEXATION

The City anticipates completing annexation of major portions of the Orcutt Area soon after adoption of the OASP. Pre-annexation agreements between the City and the landowners are a tool that may be used to detail City services that are available to Orcutt Area properties. Annexation agreements can address such issues as development entitlements, continuation of existing uses, stormwater facilities, water supply and wastewater facilities. Pre-annexation agreements are not as detailed as development agreements, and normally cannot provide for any exceptions or entitlements that would not otherwise be available to the property being annexed, however, they can clarify assumptions about the property and reduce the potential for misunderstandings in the future.

9.3 ZONING BOUNDARIES AND SUBDIVISION

The Orcutt Area will be zoned consistent with the land uses identified by Figure 1.3, the Specific Plan Map. The “SP” overlay will be added to the City zone category applied to each property. City zoning designations will take effect upon annexation.

The exact location of streets, utilities, and boundaries of development areas will be finalized as subdivision maps and development plans are generated. The designated residential zone boundaries may be adjusted slightly to reflect subdivision maps as they are approved provided that the Community Development Director makes a finding that the adjustment is consistent with the intent of the Specific Plan.

9.4 PHASING

The development of the Orcutt Area will be phased consistent with the City’s Growth Management Regulations. The City’s current Residential Growth Management Phasing Schedule, Adopted April 2006 is included in **Appendix B** for reference. Figure 9.1 identifies the areas anticipated to make up the three major OASP development phases. These phases were developed according to the need for orderly development and provision of services. They represent a reasonable approach to extending services and infrastructure throughout the Orcutt Area. In some cases, property owners may wish to develop ahead of

a property identified in Figure 9.1 as being located in an earlier phase. This may be permitted provided the necessary infrastructure to serve the proposed development is already in place, or if the project is able to construct the required infrastructure prior to development.

9.5 ARCHITECTURAL REVIEW

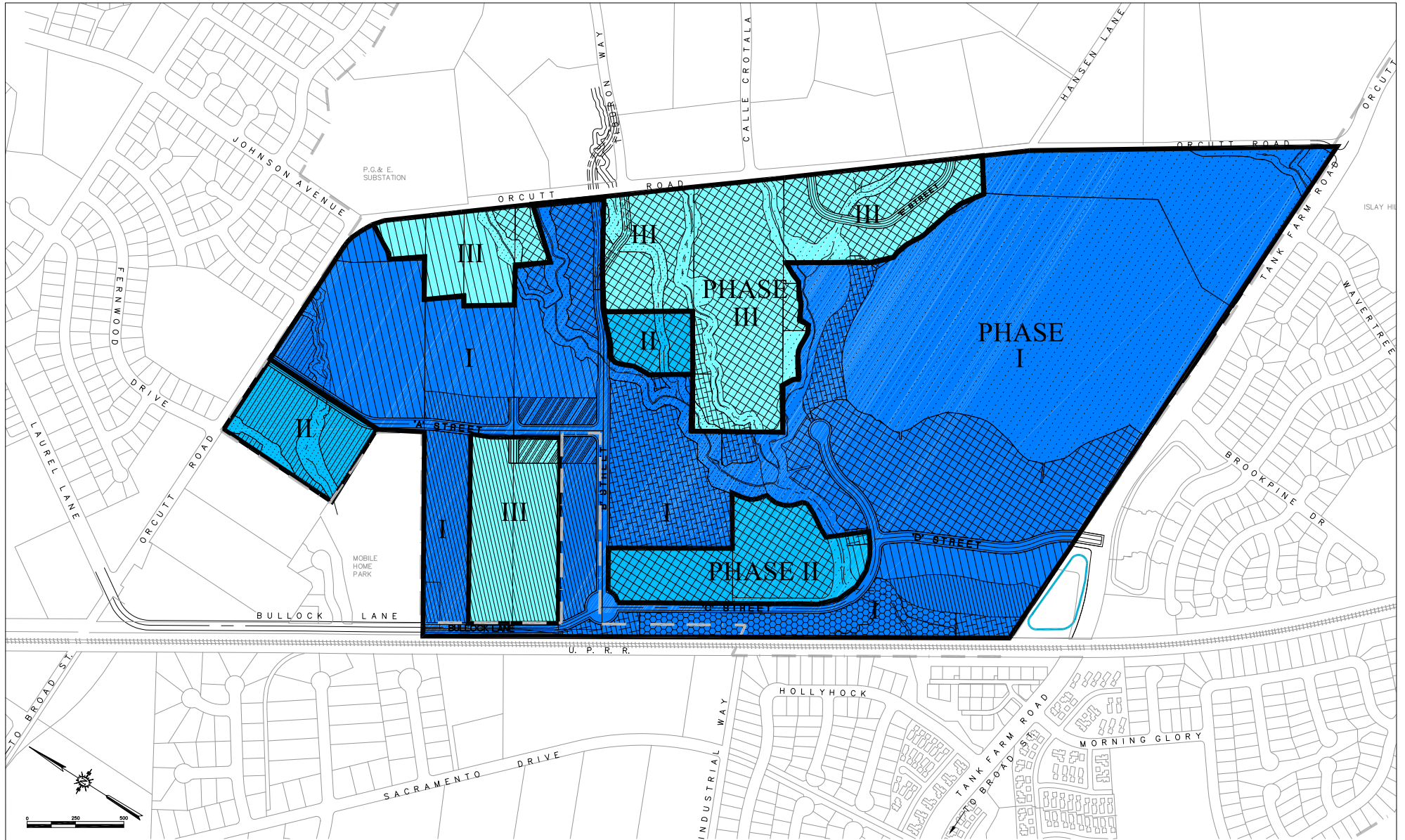
Commercial, institutional, multi-family residential and single-family tract construction will undergo architectural review per City requirements. Individual single-family dwellings are not subject to architectural review, unless they are located on “sensitive sites.” For projects subject to architectural review, the “minor or incidental” procedure should be used for those projects meeting this Specific Plan’s design standards.

9.6 BUILDING PERMITS

The City building permit process of plan-check, inspection and occupancy release will typically be the final and most detailed step in City review of private site development. Impact fees are due at the time building permits are issued.

9.7 TRANSFER OF DEVELOPMENT CREDIT/DENSITY CREDITS (TDC)












The TDC program was developed to concentrate urban development inside the City’s adopted urban reserve line, while protecting open space outside the line for agriculture, scenery, wildlife habitat, and urban buffer. To date, the TDC program has not been implemented and therefore is deemed to have no effect on the Specific Plan.



AUGUST 2009

ORCUTT EXPANSION AREA
SPECIFIC PLAN

LEGEND

 PHASE I	 R1	 R4	 MIXED USE
 PHASE II	 R2	 OPEN SPACE	 DETENTION BASIN
 PHASE III	 R3	 PARKLAND	

NOTE: PHASING FOLLOWS PROPERTY LINES OR NATURAL FEATURES

PHASING PLAN

9.8 CONSTRUCTION AND MAINTENANCE OF REQUIRED IMPROVEMENTS

Public facilities required to serve Orcutt Area development will be funded by the Orcutt Area Fee Program, as discussed in Chapter 8. Property in the Orcutt Area that is annexed into the City will receive the same public services as other neighborhoods in the City, including police, fire, and street maintenance, as described in Chapter 7. Facilities such as local streets, utility lines, park components, and stormwater facilities will be constructed by the developers of the area, and dedicated to the City upon completion and inspection. Once public facilities are constructed and the dedication is accepted by the City, future maintenance will be managed by the responsible City department.

9.9 AMENDMENT TO THE SPECIFIC PLAN

The OASP will be implemented over a period of 15 or more years. The OASP may not solve or respond to all potential questions that may surface as development in the area progresses. There may also be the desire to develop some features differently from the original proposals. Interpretations of existing policy in the Orcutt Area may be made by the Community Development Director from time to time to clarify the intent of a policy or standard and insure consistency in its implementation over time. Interpretations may be appealed to the Planning Commission by any affected person. The Specific Plan Amendment process will be used

Interpretations

Interpretations are generally limited to details where the features of the plan appear to conflict with other features in the plan or with adopted City policy. Interpretation may be necessary during discretionary development application (such as subdivision map) or ministerial development applications (such as building permits). The Community Development Director is responsible for interpretations made relative to OASP policies and standards to insure consistency in implementation as development progresses. Interpretations made by the Community Development Director are subject to appeal.

Adjustments

Adjustments are minor changes to specific features of the OASP that do not significantly alter the development type and still meet the intent of the Plan. The OASP allows for refinement of Plan features provided that any change is clearly consistent with the relevant goals, policies, programs and standards. The City anticipates that street and bicycle path locations may be slightly modified through approval of subdivision maps, and zoning boundaries may also be modified to match new property lines created through the subdivision process.

Amendments

Amendments are changes to the OASP involving differences in the type or capacity of development or public services. Examples of changes requiring amendment include:

- 1) Eliminating or reclassifying major streets;
- 2) Increasing the number of street intersections on Orcutt Road or Tank Farm Road;
- 3) Eliminating any segment of the bicycle-pedestrian circulation system (unless replaced with a new segment providing equally convenient access);
- 4) Changing the proposed cross-section design of streets so that one or more components are eliminated (such as sidewalks, parking, landscaped parkway, or bikeways);
- 5) Changing zoning on a parcel as indicated in the Specific Plan;
- 6) Significantly altering residential densities from the proposed range; or

7) Significantly altering the regional stormwater detention basin design or its capacity.

Amendments will involve a question of consistency with the original intent of the OASP or with the General Plan. Amendments require a hearing and recommendation by the Planning Commission, with final action to be taken by the City Council.

APPENDIX A

A-1 ORCUTT AREA OWNERSHIP AND DEVELOPMENT POTENTIAL

Table A-1: Orcutt Area Property Owners, and **Figure A-1:** Ownership Map. The Orcutt Area includes 230.85 acres owned by 13 landowners, currently divided into 21 parcels. Each parcel has unique development potential based on natural and existing constraints (hillsides, creeks, riparian vegetation, railroad and arterial noise buffers, etc.). The OASP is designed to develop a new residential neighborhood in San Luis Obispo while considering these constraints and providing amenities for the future residents of the Orcutt Area (such as parks, open space, recreational trails, roads, detention, etc.).

Table A-2: Development Potential by Landowner. This table shows the estimated potential number of residential units for each landowner (expressed in a range of units). The residential development potential for each landowner is also expressed as number of acres and units for each development type in this plan (R-1, R-2, R-3, R-4, and commercial). The table also provides estimates of developable land for each landowner (land that does not have any natural constraints such as hillside or creeks) and shows the percent of developable land to the total developable land in the OASP as a percentage. It should be noted that the values included in this table are estimates and the exact final areas will not be known until future subdivisions are reviewed and approved. Development may be constrained by factors unknown at the time the OASP was developed, including the presence of biological resources, cultural resources or other factors.

TABLE A -1
ORCUTT AREA PROPERTY OWNERS

Property Owner Name/ Company Name	Assessors Parcel Numbers	Street Address (mailing)	City, State, Zip Code
Jeanne Anderson	076-481-008	PO Box 688	Wilderville, OR 97543
Leo and Margaret Evans	076-491-001	2248 Glacier Lane	Santa Maria, CA 93455
Jack and Lorene Farrior	076-481-015	1495 Orcutt Road	San Luis Obispo, CA 93401
Paul Fiala	076-481-012	3745 Orcutt Road	San Luis Obispo, CA 93401
Paul Garay	076-491-004	3811 Orcutt Road	San Luis Obispo, CA 93401
Mid-State Properties Phil Gray	076-481-002	1320 Archer Street	San Luis Obispo, CA 93401
Garney and Vicky Hall	076-481-014	1955 Oak Way	Arroyo Grande, CA 93420
Phyllis Imel	076-491-003	3777 Orcutt Road	San Luis Obispo, CA 93401
Ernest Jones Peppertree Properties	076-481-011	3765 Orcutt Road	San Luis Obispo, CA 93401
Maddalena Family	076-481-001	1329 Orcutt Road	San Luis Obispo, CA 93401
Nick Muick Patti Taylor Rick Taylor	076-481-017 076-481-016	PO Box 12302	San Luis Obispo, CA 93406
Righetti Land Principal Landholders: Barbara Parsons (29.75%) Jeanne Helphenstine (40.5%) Gary Esajian (29.75%)	076-491-002 076-411-046 044-011-039 076-491-008 076-491-005 053-091-016	4650 Portola Road	Atascadero, CA 93422
Chuck Pratt Sandpiper Investments, Inc.	053-061-024 076-481-007 076-481-006	PO Box 1877	San Luis Obispo, CA 93406

**TABLE A-2
ORCUTT AREA SPECIFIC PLAN
DEVELOPMENT POTENTIAL BY LANDOWNER¹**

LAST NAME	ASSESSOR NO.	TOTAL ESTIMATED UNITS ² (Residential)	ACRES OWNED		R-1 DEVELOPMENT		R-2 DEVELOPMENT ⁵		R-3 DEVELOPMENT		R-4 DEVELOPMENT		COMMERCIAL		TOTAL DEVELOPMENT ¹		DEVELOPMENT PERCENTAGE
			Acres	% of Total Acres	Acres	Units ⁵	Acres	Units ⁵	Acres	Units ⁵	Acres	Units ⁵	Acres	Sq. Feet	Acres of Dev/pt.	% of Total Dev/pt.	
ANDERSON	076-481-008	68-77	5.35	2.3%	0.00	0	0.00	0	5.03	68-77	0.00	0	0.00	0	5.03	4.4%	94.0%
EVANS	076-491-001	70-80	5.62	2.4%	0.00	0	0.00	0	5.23	70-80	0.00	0	0.00	0	5.23	4.5%	93.1%
FARRIOR	076-481-015	7-8	0.76	0.3%	0.00	0	0.76	7-8	0.00	0	0.00	0	0.00	0	0.76	0.7%	100.0%
FIALA	076-481-012	3-4	0.95	0.4%	0.71	3-4	0.00	0	0.00	0	0.00	0	0.00	0	0.71	0.6%	74.7%
GARAY	076-491-004	43-45	13.70	5.9%	11.18	43-45	0.00	0	0.00	0	0.00	0	0.00	0	11.18	9.7%	81.6%
HALL	076-481-014	7-11	1.13	0.5%	0.00	0	1.13	7-11	0.00	0	0.00	0	0.00	0	1.13	1.0%	100.0%
IMEL	076-491-003	16-17	6.55	2.8%	4.24	16-17	0.00	0	0.00	0	0.00	0	0.00	0	4.24	3.7%	64.7%
JONES	076-481-011	54-56	11.63	5.0%	2.84	10-11	4.75	44-45	0.00	0	0.00	0	0.00	0	7.59	6.6%	65.3%
MADDALENA	076-481-001	79-89	6.66	2.9%	0.00	0	0.00	0	0.00	0	4.68	79-89	0.00	0	4.68	4.1%	70.3%
MID-STATE	076-481-002	64-68	11.75	5.1%	0.00	0	8.48	52-54	0.00	0.00	0.72	12-14	0.00	0	9.20	8.0%	78.3%
MUICK	076-481-017	72-74	12.00	5.2%	0.00	0.00	9.41	72-74	1.45	19-22	0.00	0	0.00	0	10.86	9.4%	90.5%
TAYLOR	076-481-016	288-313	143.81	62.3%	34.32	181-187	8.11	69-84	2.81	38-42	0.00	0	0.00	0	45.24	39.3%	31.5%
PARSONS	076-411-046 044-011-039																
	076-491-008 076-491-005 053-091-016																
PRATT	053-061-024	49-55	5.41	2.3%	0.00	0	0.00	0	3.66	49-55	0.00	0	0.25	11,000	3.91	3.4%	72.3%
PRATT	076-481-007 076-481-006	72-82	5.55	2.4%	0.00	0	0.00	0	5.34	72-82	0.00	0	0.00	0	5.34	4.6%	96.2%
TOTALS		892-979	230.87	100.0%	53.29	253-254	32.64	251-276	23.52	297-336	5.40	91-103	0.25	11,000	115.09	100.0%	49.9%

***Total estimated number of units is based on the zoning and statistical information provided by the property owners. These acreages and density figures were based on assumptions and used for planning purposes. Actual development area and number of dwelling units will vary.

- All areas and percentages presented here are based on scaled dimensions from maps available and are subject to variation.
- The number of units is based on an estimated range of: 3-6 du/acre for R-1; 5-12 du/acre for R-2; 18 du/acre for R-3 with 75-85% of the area considered developable; and 24 du/acre for R-4 with 70-80% considered net area available for development.
- The 4.62 acre R-2 area west of the park site on the Righetti property was originally planned to be used as a school site by the San Luis Coastal School District. The School District may be pursuing other, similarly sized sites within the Orcutt Area, as discussed in Chapter 3.2.6.
- Total Development includes area available for residential and commercial development, as well as roads (other than those shown on Specific Plan Figure 2). It also includes the areas under the power corridor in the northern portion of the Plan Area.
- Unit counts for each property have been based on theoretical subdivision designs as actual subdivision designs were not available.



FIGURE A-1
OWNERSHIP MAP

ORCUTT EXPANSION AREA
SPECIFIC PLAN

A-2 INDIVIDUAL PROPERTIES – Special Considerations

A distinguishing factor of the Orcutt Expansion Area is property ownership by 13 different private parties. Differences in individual owner’s needs and goals have been addressed within the body of the Orcutt Area Specific Plan to the fullest extent possible.

A-2-1 RESIDENTIAL HOME SITE PRESERVATION

It is the intent of several of the Orcutt Area property owners to preserve their existing home sites from requirements resulting from adjacent development on either a portion of their own property, or on neighboring property (**Figure A-2**). The concept of a “home site” was developed early in the planning stages for the Orcutt Area to address the concerns of many property owners with different long term goals, and different existing uses on their property.

The City’s experience with the Airport Area annexation has shown that these “home site” policies are an important aspect of a successful annexation process. Property owners in annexation areas want protection from new fees and expenses, such as impact fees for water and sewer service until such time as they are ready and able to develop. As a result, the City’s practice has been to allow for existing uses of on-site water resources and septic systems to continue until such time as new development is proposed. The City’s Zoning Regulations also allows existing uses that were legally established to continue indefinitely upon annexation, even if they are non-conforming. However, there are limitations on the expansion of non-conforming uses, and all new development must meet City standards.

The OASP includes the following language to address Orcutt Area property owners concerns regarding these issues.

*Policy A-2a: Property owners may designate a “home site” area over a portion or all of their property. The currently designated “home sites” are identified in **Figure A-2**, which may be updated from time to time.*

Policy A-2b: When a property owner, or their heirs, wish to remove the “home site” designation, such termination will be an administrative function which does not require amendment to the Specific Plan, or special approval by any City decision making body, or any other applicable agency.

Program A-2.1: The designation of “home sites” was used during the planning process for the OASP to address property owners concerns over the potential for development of streets or utilities on or through existing home sites as a result of implementation of the OASP. The OASP has been designed to avoid such conflicts. The OASP also recognizes an owner’s ability to set aside a “home site” until such time as the designated owner, or their heirs, withdraw this designation. Appendix A, Section A-2-2, describes each property owner’s intent with respect to home site considerations, which has been considered during the planning process for the OASP.

Program A-2.2: For all property in the Orcutt Area, impact fees will not be incurred until the property is developed. Impacts fees will not be due until building permits are pulled for development.

ORCUTT AREA SPECIFIC PLAN

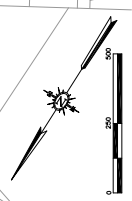
Appendix A

- Program A-2.3: Consistent with Chapter 17.10 of the Zoning Regulations, existing non-conforming uses may be allowed to continue indefinitely. Change of ownership, tenancy or management of a non-conforming use shall not affect its legal, nonconforming status.
- Program A-2.4: Non-conforming uses in the Orcutt Area may include existing agricultural operations and activities, existing businesses, or existing residential uses that do not meet the standards adopted as part of the OASP.



LEGEND - PROPOSED LAND USE
 [Hatched Box] HOME SITE LOCATION
 [Dashed Line] CITY LIMIT
 [Dash-Dot Line] ORCUTT AREA BOUNDARY

ORCUTT EXPANSION AREA
 SPECIFIC PLAN



AUGUST 2009

FIGURE A-2
 HOME SITE EXHIBIT

A-2-2 Individual Property Development Guidelines

The Specific Plan has identified owner's wishes for their property, in some cases showing minimal on-site improvements at the owner's request. The OASP will serve as a guide for orderly development, therefore, roads have been located so that access and utilities can be made available to all properties. However, the impact of these roads and utilities has been minimized to the greatest extent possible where it would conflict with a property owner's expressed desire to maintain a home site. The home site considerations that were addressed during the planning process for the Orcutt Area are described below.

Anderson

The owner intends to hold the western portion of the total property acreage as "home site." While the entire property will be designated for medium-high density residential development, the owner may choose to preserve the "home site" area and develop the remainder of the property.

In order to construct the extension of Bullock Lane to the traffic circle intersection, a portion (approximately 12 feet of the southwest boundary) of the Anderson property shall be dedicated in Phase I to allow for construction of the Class I bicycle path and two through lanes. The balance of the roadway dedication will occur at the time of development.

Evans

The entire property will be designated for medium-high density residential development.

In order to construct the extension of Bullock Lane to the traffic circle intersection, a portion (approximately 12 feet of the southwest boundary) of the Evans property shall be dedicated in Phase I to allow for construction of the Class I bicycle path and two through lanes. The balance of the roadway dedication will occur at the time of development.

Farrior

The property owner has chosen a medium density residential land use designation. The owner has designated the entire property as a "home site."

Fiala

The property owner has chosen a medium density residential land use designation. The owner has designated the entire property as a "home site."

Garay

The owner has designated the entire property as a "home site." The current property owners have no plans to develop. They have not approved of any plans for development; therefore, none are shown in this Specific Plan. They have agreed, however, for environmental review, to a land use designation of low-density residential/R-1 zoning. They have strongly rejected any level of more detailed planning at this time.

Development costs and fees per unit as set forth in the Appendix of this Specific Plan do not reflect any development on this property and calculations of park acreage allowance for the Orcutt Area development did not take into account any units on the Garay Property.

If the present or future landowner chooses to develop, then the property will then be subject to OASP policies and programs including:

1. Providing open space and creek setbacks;
2. Dedicating park acreage related to the number of residential units developed;
3. Paying infrastructure costs and fees for each unit developed at rates established in the Specific Plan;

4. Providing a local street to serve their property and provision of local access to the remnant of the Parsons property to the south.

Hall

The property owner has chosen a medium density residential land use designation. The owner has designated the entire property as a “home site.”

Imel

The owner intends to maintain the whole property as a “home site.” While a low-density residential designation has been accepted for the property, the property owners do not approve of any plans for development; therefore, none is shown in this Specific Plan. They have agreed, however, for environmental review, to a land use designation of low-density residential/R-1 zoning. They have strongly rejected any level of more detailed planning at this time.

To accommodate this owner’s stated intentions, an alternative alignment of ‘B’ Street has been designed. The decision regarding the final alignment of ‘B’ Street shall be made in the future, when Orcutt Area development requires construction of this street connection to Orcutt Road.

Jones

The current property owner has chosen to designate the portion of the property on the east side of the creek area as low density residential. The western portion of the property is designated medium density residential consistent with the adjacent properties to the north.

Mid State Properties

The current property owner intends to develop the northwestern corner of the property for a high-density residential land use consistent with the land use indicated for the adjacent parcel to the west. The remainder of the Mid-State property is designated for medium density residential land use. Consideration of a graduated care facility is reserved for the Mid State property.

Muick

The owner intends to hold the northeastern portion of the total property acreage as a “home site.” While the entire property will be designated for medium density residential development, the owner may choose to preserve the “home site” area while developing the remainder of the property.

Parsons

The Righetti Family will designate a 15.36-acre parcel of their property, which includes their family home, as open space under a conservation easement. (See Chapter 2 of this Specific Plan.)

Pratt

The owner’s three parcels located on Bullock Lane have been designated medium-high density residential land use with 0.5 acres of the owner’s southern parcel to be developed as mixed use-community commercial. The owner intends to hold the southwestern portion of the total property acreage of the parcel located farthest to the south as “home site.” While the entire property will be designated for medium-high density residential development, the owner may choose to preserve the “home site” area while developing the remainder of the property.

In order to construct the extension of Bullock Lane and the traffic circle intersection, the necessary land should be dedicated in Phase I.

ORCUTT AREA SPECIFIC PLAN Table A-3
AIRPORT LAND USE PLAN - CONSISTENCY EVALUATION
DENSITY BY PROPERTY*

LAST NAME/ PROPERTY OWNER	ASSESSOR NO.	ESTIMATED UNITS (Residential)	ACRES OWNED (Residential)	GROSS DENSITY ² (UNITS/AC)	EFFECTIVE DENSITY WITH 25% AFFORDABLE HOUSING BONUS (Gross Density * 25%)
ANDERSON	076-481-008	68-77	5.35	14.39	17.99
EVANS	076-491-001	70-80	5.62	14.23	17.79
FARRIOR	076-481-015	7-8	0.76	10.52	13.16
FIALA	076-481-012	3-4	0.95	4.21	5.26
GARAY	076-491-004	43-45	13.70	3.28	4.10
HALL	076-481-014	7-11	1.13	9.73	12.10
IMEL	076-491-003	16-17	6.55	2.59	3.24
JONES	076-481-011	54-56	11.63	4.81	6.02
MADDALENA	076-481-001	79-89	6.66	13.30	16.70
MID-STATE	076-481-002	64-68	11.75	5.78	7.23
MUICK	076-481-017	72-74	12.00	6.17	7.72
TAYLOR	076-481-016				
PARSONS	076-491-002 076-411-046 044-011-039 076-491-008 076-491-005 053-091-016	288-313	95.99 ¹	3.26	4.07
PRATT	053-061-024	49-55	5.41	10.10	12.70
PRATT	076-481-007 076-481-006	72-82	5.55	14.77	18.46 ³
TOTALS		892-979	87.06	8.36	10.47

- * In no case may the density on any property exceed 18 units/acre as calculated by the provisions of the Airport Land Use Plan
1. This acreage, 95.99 AC, reflects the total acreage on Parsons property (143.81AC) less Righetti Hill (47.82 AC).
 2. Maximum Estimated units used to calculate gross density.
 3. Exceeds Airport Land Use Plan limit of 18 units/acre. Maximum number of units allowed = 18 * 5.55 acres = 99 units

APPENDIX B: Residential Growth Management Ordinance Phasing Schedule *

		Number of Dwellings Permitted (a, b)								2002 - 2022
calendar years:		1999-2001	2002-04	2005-07	2008-10	2011-13	2014-16	2017-19	2020-22	Total
assumed	demolitions	-40	-40	-60	-75	-80	-50	-50	-50	-445
assumed	new in-city (c)	290	110	350	140	140	140	100	100	1,370
allowed	Dalidio	0	0	0	0	60	0	0	0	60
allowed	Irish Hills North	190	80	0	0	0	0	0	0	270
allowed	Irish Hills South	53	70	0	0	0	0	0	0	123
allowed	Orcutt	0	0	0	170	200	360	270	0	1,000
allowed	Margarita	0	0	80	368	300	120	0	0	868
assumed	other annexations	5	10	5	5	5	5	5	5	45
calculated	Interval total:	498	230	375	608	625	575	325	55	3,291
	average annual % change (d)	0.85	0.40	0.64	1.00	1.00	0.90	0.50	0.08	0.55 (e)

- (a) Dwellings affordable to residents with very low, low, or moderate incomes, as defined in the Housing Element, are exempt.
- (b) This is a simple count of dwellings and is not meant to reflect the Zoning Regulation's method for calculating fractional dwellings.
- (c) Includes the incorporated area in 1994 and certain annexations during 1994 - 1998 (Stoneridge; Prefumo Homes; and the El Capitan, Goldenrod, and Fuller Road parts of the Edna-Isly Specific Plan, which has its own growth management provisions).
- (d) A calculated result: dwellings permitted (new construction minus demolitions), divided by 3, divided by the total number of dwellings projected to be in the city at the middle of the interval, X 100; assumes that the maximum amounts are achieved in previous intervals.
- (e) A calculated result: the compound growth rate that over 24 years would result in the total net increase.

*Although this phasing schedule is no longer part of the Growth Management Ordinance, it is reprinted here because it served as the basis for the Facility Cost Phasing shown in OASP Table 8.10 and Table 8.11.

APPENDIX C - MITIGATION MEASURES FOR CLASS II IMPACTS

The following mitigation measures are required for the implementation of the Specific Plan and are direct quotes from the Environmental Impact Report. All figure references are from the Environmental Impact Report. Note that numbering may not always be sequential as some impacts are either not mitigable (Class I) or less than significant (Class III).

AIR QUALITY

AQ-1 Vehicular operations associated with development under the Specific Plan would result in the emission of levels of air pollutants that would exceed recommended significance thresholds and are therefore considered to have a Class II, significant but mitigable, impact.

The Specific Plan includes bikeways, pedestrian walkways, and access to public transit routes that will reduce the need for vehicle transportation and therefore reduce the amount of emissions (Specific Plan Section 5.3 and associated policies and programs). The Specific Plan also encourages the use of solar energy sources for residential and commercial uses (Specific Plan Policies 4.7.1 and 4.7.2). Finally, bike lanes have been designed to provide continuous connections through the Specific Plan area, consistent with regional goals related to reducing dependence on motorized vehicle travel.

The following standard site design and discretionary energy efficiency mitigation measures are recommended:

- AQ-1(a) Energy Efficiency.** The building energy efficiency rating shall be 10% above what is required by Title 24 requirements for all buildings within the Specific Plan Area. The following energy-conserving techniques shall be incorporated unless the applicant demonstrates their infeasibility to the satisfaction of City Planning and Building Department staff: increase walls and attic insulation beyond Title 24 requirements; orient buildings to maximize natural heating and cooling; plant shade trees along southern exposures of buildings to reduce summer cooling needs; use roof material with a solar reflectance value meeting the Environmental Protection Agency/Department of Energy Energy Star rating; build in energy efficient appliances; use low energy street lighting and traffic signals; use energy efficient interior lighting; use solar water heaters; and use double-paned windows.
- AQ-1(b) Transit.** Bus turnouts and shelter improvements with direct pedestrian access shall be installed at all bus stops.
- AQ-1(c) Shade Trees.** All parking lots shall include shade trees within the parking area. There shall be at least one shade tree for every six vehicle parking spaces.

- AQ-1(d) Telecommuting.** All new homes within the Specific Plan area shall be constructed with internal wiring/cabling that allows telecommuting, teleconferencing, and telelearning to occur simultaneously in at least three locations in each home.
- AQ-1(e) Pathways.** Where feasible, all cul-de-sacs and dead-end streets shall be links by pathways to encourage pedestrian and bicycle travel.
- AQ-1(f) Pedestrian Signalization.** All new signalized intersections shall include signalization to accommodate pedestrian crossings. Pedestrian signalization shall allow pedestrians to call for a traffic signal change.

AQ-3 Development under the proposed Specific Plan has the potential to generate construction related emissions as the site develops. Although these emissions cannot be quantified at the Program EIR level, since San Luis Obispo County is currently non-attainment for PM10, development under the Specific Plan would contribute to this existing significant condition. Therefore, construction related emissions are considered to be Class II, significant but mitigable.

Because all construction projects can produce nuisance dust emissions, dust mitigation measures are required for all construction activities. The following mitigation measures are recommended to minimize emissions and to reduce the amount of dust that drifts onto adjacent properties. These measures would apply to both tract grading and development of individual lots.

- AQ-3(a) Application of CBACT (Best Available Control Technology for construction related equipment).** The following measures shall be implemented to reduce combustion emissions from construction equipment where a project will have an area of disturbance greater than 1 acre.
- Specific Plan applicants shall submit for review by the Community Development Department and Air Pollution Control District (APCD) staff a grading plan showing the area to be disturbed and a description of construction equipment that will be used and pollution reduction measures that will be implemented. Upon confirmation by the Community Development Department and APCD, appropriate CBACT features shall be applied. The application of these features shall occur prior to Specific Plan construction.
 - Specific Plan applicants shall be required to ensure that all construction equipment and portable engines are properly maintained and tuned according to manufacturer's specifications.
 - Specific Plan applicants shall be required to ensure that off-road and portable diesel powered equipment, including but not limited to bulldozers, graders, cranes, loaders, scrapers, backhoes, generator sets, compressors, auxiliary power units, shall be fueled exclusively with CARB motor vehicle diesel fuel (non-taxed off-road diesel is acceptable).
 - Specific Plan applicants shall be required to install a diesel oxidation catalyst on each of the two pieces of equipment projected to generate the

greatest emissions. Installations must be prepared according to manufacturer's specifications.

AQ-3(b) Dust Control. The following measures shall be implemented to reduce PM10 emissions during all Specific Plan construction:

- Reduce the amount of the disturbed area where possible.
- Use water trucks or sprinkler systems in sufficient quantities to prevent airborne dust from leaving the site. Water shall be applied as soon as possible whenever wind speeds exceed 15 miles per hour. Reclaimed (nonpotable) water should be used whenever possible.
- All dirt-stock-pile areas shall be sprayed daily as needed.
- Permanent dust control measures shall be identified in the approved Specific Plan revegetation and landscape plans and implemented as soon as possible following completion of any soil disturbing activities.
- Exposed ground areas that are planned to be reworked at dates greater than one month after initial grading shall be sown with a fast-germinating native grass seed and watered until vegetation is established.
- All disturbed soil areas not subject to revegetation shall be stabilized using approved chemical soil binders, jute netting, or other methods approved in advance by the APCD.
- All roadways, driveways, sidewalks, etc., to be paved shall be completed as soon as possible. In addition, building pads shall be laid as soon as possible after grading unless seeding or soil binders are used.
- Vehicle speed for all construction vehicles shall not exceed 15 mph on any unpaved surface at the construction site.
- All trucks hauling dirt, sand, soil or other loose materials shall be covered or shall maintain at least two feet of freeboard (minimum vertical distance between top of load and top of trailer) in accordance with CVC Section 23114.
- Install wheel washers where vehicles enter and exit unpaved roads onto streets, or wash off trucks and equipment leaving the site.
- Sweep streets at the end of each day if visible soil material is carried onto adjacent paved roads. Water sweepers with reclaimed water shall be used where feasible.

AQ-3(c) Cover Stockpiled Soils. If importation, exportation, or stockpiling of fill material is involved, soil stockpiled for more than two days shall be covered, kept moist, or treated with soil binders to prevent dust generation. Trucks transporting material shall be tarped from the point of origin.

AQ-3(d) Dust Control Monitor. On all projects with an area of disturbance greater than 1 acre, the contractor or builder shall designate a person or persons to monitor the dust control program and to order increased watering as necessary to prevent transport of dust off-site. Their duties shall include holiday and weekend periods when work may not be in progress.

BIOLOGICAL RESOURCES

- B-3** Development under the proposed Specific Plan could affect locally-designated protected trees. This is considered a Class II, significant but mitigable impact.

The proposed Specific Plan includes a program which is intended to address potential impacts associated with this issue. In addition, the applicants under the Specific Plan will be required to comply with the City's Tree Regulations (City of San Luis Obispo, 1997). The following mitigation measure is also required to ensure compliance with the City's Tree Regulations and to reduce potential impacts to trees to a less than significant level.

- B-3(a) Construction Requirements.** Development under the Specific Plan shall abide by the requirements of the City Arborist for construction. Requirements shall include but not be limited to: the protection of trees with construction setbacks from trees; construction fencing around trees; grading limits around the base of trees as required; and a replacement plan for trees removed including replacement at a minimum 1:1 ratio.

- B-4** Development under the proposed Specific Plan would affect riparian woodland and wetland habitat. This is considered a Class II, significant but mitigable impact.

The Specific Plan has incorporated goals, policies, and programs to alleviate impacts to biological resources. The following mitigation measures are also required to assure compliance with the City's Creek Setback Ordinance (Section 17.16.025 of the City's Zoning Regulations) and reduce impacts to riparian and wetland habitat to a less than significant level. Mitigation measures from Section 4.6, Drainage and Water Quality, would further reduce potentially significant impacts to wetlands. Also refer to Mitigation Measures under Impact B-5 that apply to setbacks with respect to special-status species

- B-4(a) Trail Setbacks.** Trails shall be setback out of riparian habitat and out of the buffer area. The trail shall be a minimum distance of 20 feet from top of bank or from the edge of riparian canopy, whichever is farther. Trails shall be setback from wetland habitat at a minimum distance of 30 feet and shall not be within the buffer. Native plant species that will deter human disturbance shall be planted in the area between the trail and the wetland/riparian habitat including plants such as California rose (*Rosa californica*) and California blackberry (*Rubus ursinus*). No passive recreational use shall be allowed in the riparian or wetland habitats or drainage corridors.

- B-4(b) Development Setbacks.** Development that abuts riparian and wetland mitigation areas shall also be setback at least 20 feet, and be buffered by an appropriately-sized fence and/or plants that deter human entry listed in B-4(a).

- B-4(c) Riparian/ Wetland Mitigation.** If riparian and/or wetland habitat are proposed for removal pursuant to development under the Specific Plan, such development shall apply for all applicable permits and submit a Mitigation Plan for areas of disturbance to wetlands and/or riparian habitat. The plan

shall be prepared by a biologist familiar with restoration and mitigation techniques. Compensatory mitigation shall occur on-site using regionally collected native plant material at a minimum ratio of 2:1 (habitat created to habitat impacted) in areas shown on Figure 4.4-2 as directed by a biologist. The resource agencies may require a higher mitigation ratio. If the Orcutt Regional Basin is necessary as a mitigation site for waters of the U.S. and State it shall be designed as directed by a biologist taking into consideration hydrology, soils, and erosion control and using the final mitigation guidelines and monitoring requirements (U.S. Army Corps of Engineers, 2004). As noted above, the trail shall be setback out of the buffer area for riparian and wetland habitat.

- B-5** Development under the proposed Specific Plan could potentially impact special-status wildlife species and their habitats within the Plan Area. This is considered a Class II, significant but mitigable impact.

The Specific Plan establishes permanent open space for the creek area, and when combined with the buffering setbacks required by the City, impacts would be reduced substantially. Compliance with Federal and State regulations governing the wetland and riparian habitat types on-site (described in Impact B-3) would also reduce impacts to these important biological resources. Specific Plan policies would also require any development proposal pursuant to the Specific Plan that would remove riparian or wetland areas to mitigate for such impacts. However, the following additional mitigation measures are required to reduce impacts to all special-status wildlife species to a less than significant level.

B-5(a) Bird Pre-Construction Survey. To avoid impacts to nesting special-status bird species and raptors including the ground-nesting burrowing owl, all initial ground-disturbing activities and tree removal shall be limited to the time period between September 15 and February 1. If initial site disturbance, grading, and tree removal cannot be conducted during this time period, a pre-construction survey for active nests within the limits of grading shall be conducted by a qualified biologist at the site two weeks prior to any construction activities (for ground-nesting burrowing owl survey see below). If active nests are located, all construction work must be conducted outside a buffer zone of 200 feet to 500 feet from the nests as determined in consultation with the (California Department of Fish and Game (CDFG)). No direct disturbance to nests shall occur until the adults and young are no longer reliant on the nest site. A qualified biologist shall confirm that breeding/nesting is completed and young have fledged the nest prior to the start of construction.

B-5(b) Burrowing Owl Survey. When an applicant requests entitlements from the City under the Specific Plan a qualified biologist shall conduct surveys for burrowing owls during both the wintering and nesting seasons (unless the species is detected on the first survey) in potentially suitable habitats prior to construction in accordance with the guidelines described in the CDFG Staff Report on Burrowing Owl Mitigation (1995). Winter surveys shall be conducted on the entire project site between December 1 and February 1, and the nesting season survey shall be conducted between April 15 and July 15.

If burrowing owls are detected within the proposed disturbance area, CDFG shall be contacted immediately to develop and implement a mitigation plan to protect owls and their nest sites.

- B-5(c) Monarch Pre-Construction Survey.** If initial ground-breaking is to occur between the months of October and March a pre-construction survey for active monarch roost sites within the limits of grading shall be conducted by a qualified biologist at the site two weeks prior to any construction activities. If active roost sites are located no ground-disturbing activities shall occur within 50 feet of the perimeter of the habitat. Construction shall not resume within the setback until a qualified biologist has determined that the monarch butterfly has vacated the site.
- B-5(d) VPFS Sampling Surveys.** Prior to development in areas shown as potential vernal pool fairy shrimp (VPFS) habitat on Figure 4.4-2, current United States Federal Wildlife Service (USFWS) protocol level sampling surveys shall be conducted in all such areas. A report consistent with current Federal, State, and local reporting guidelines shall be prepared to document the methods and results of surveys. If VPFS are found, the report shall include a map that identifies the VPFS locations. Should the presence of additional special-status wildlife species be determined including California linderiella, a map identifying locations in which these species were found shall be prepared and included in the report.
- B-5(e) FESA Consultation and Mitigation Regarding VPFS.** If any VPFS individuals are located onsite pursuant to Mitigation Measure B-5(d), substantial setbacks from their identified habitat shall be implemented to avoid take of a Federally listed species. If complete avoidance is not economically or technically feasible, then Section 10 of the Federal Endangered Species Act (FESA) shall be used to authorize incidental take when no other Federal agency such as the Corps is involved. This process includes development of a Habitat Conservation Plan for protecting and enhancing the Federally listed species at a specific location in perpetuity. Species take can also be authorized under Section 7 of the FESA if a Federal agency is involved in the project (e.g., Corps Section 404 permitting for impacts to waters of the U.S. and/or Federal funding) and agrees to be the lead agency requesting Section 7 consultation. This consultation process takes at a minimum 135 days from the official request by the Federal lead agency.

The compensatory mitigation ratio shall be determined by the appropriate resource agencies. Suitable replacement habitat shall be constructed either within the site boundaries or offsite. Figure 4.4-2 identifies areas that could be appropriate for onsite VPFS mitigation. Figure 4.4-2 is not intended to preclude development but shall be used as a starting point for incorporating VPFS mitigation sites into the development plan. While the Orcutt Regional Basin included in the potential VPFS mitigation sites may need regular maintenance and may be seasonally flooded, depressions could be created on the upper edges of the terrace in such a manner that they are protected from flooding. VPFS mitigation areas shall be approved by a biologist familiar with VPFS habitat “creation” techniques. Enhancement of the onsite seasonal

freshwater wetland habitat that is undisturbed by project activities may also be a part of the mitigation program. Alternatively, fairy shrimp cysts could be collected during the dry season from the existing habitat and placed into storage. Topsoil could also be removed and stored in conditions suitable to retain cysts. Wetland habitat could be enhanced/created in the areas shown on Figure 4.4-2 by grading depressions in the landscape and “top dressing” the depressions with the preserved topsoil. Preserved cysts would be added to the recreated wetlands in December or January, after sufficient ponding has occurred.

It is important to note that VPFS habitat mitigation is still considered experimental. VPFS habitat mitigation is ambitious as it is costly, labor intensive, and difficult to ensure success. Habitat may be “created” only in an existing vernal pool landscape that provides suitable soils and a number of other specific ecological factors (USFWS, 2004).

An alternative to onsite mitigation is the purchase of mitigation bank credits. Credits can be purchase by the acre as suitable mitigation for VPFS. There is currently no known mitigation bank with VPFS habitat occurring within San Luis Obispo County, however, mitigation banks may be available in the future.

- B-6** Development under the proposed Specific Plan would reduce the populations and available habitat of wildlife in general. The loss of wildlife habitat is considered a Class II, significant but mitigable impact.

The following mitigation measures are required to fully reduce impacts to a less than significant level.

- B-6(a) Minimized Roadway Widths.** Roadway widths adjacent to riparian and wetland habitats shall be reduced to the minimum width possible, while maintaining Fire Department Requirements for emergency access, with slower speed limits introduced. Posted speed limits should be 25 mph.
- B-6(b) Culvert Design.** Although closed culverts are to be the drainage conveyance method of last resort per the City Waterways Management Plan, where they are required, culverts connecting the Plan Area drainage corridors with upstream and downstream drainage corridors shall be evaluated during the suitability analysis pursuant to Mitigation Measure B-5(a) to determine their importance to wildlife who could use them to travel to and from the site. If culverts are found to be of importance to wildlife, the culverts shall be evaluated for their potential for improvement (i.e. retrofitting, maintenance, or specific improvements depending on the types of species using them). The development pursuant to the Specific Plan and the City shall develop a plan for the improvement of the culverts. Preservation of the wildlife corridors that are present on the project site can be achieved with sufficient setbacks from riparian and wetland habitats. Refer to B-4 for mitigation regarding riparian and wetland habitat setbacks.

B-6(c) Educational Pet Brochure. Any development pursuant to the Specific Plan shall prepare a brochure that informs prospective homebuyers and Home Owners Association (HOA) members about the impacts associated with non-native animals, especially cats and dogs, to the project site; similarly, the brochure must inform potential homebuyers and all HOA members of the potential for coyotes to prey on domestic animals.

B-6(c) Landscaping Plan Review. To ensure that project landscaping does not introduce invasive non-native plant and tree species to the region of the site, the final landscaping plan shall be reviewed and approved by a qualified biologist. The California Invasive Plant Council (Cal-IPC) maintains several lists of the most important invasive plants to avoid. The lists shall be used when creating a plant palette for landscaping to ensure that plants on the lists are not used. The following plants shall not be allowed as part of potential landscaping plans pursuant to development under the Specific Plan:

- African sumac (*Rhus lancea*)
- Australian saltbush (*Atriplex semibaccata*)
- Black locust (*Robinia pseudoacacia*)
- California pepper (*Schinus molle*) and Brazilian pepper (*S. terebinthifolius*)
- Cape weed (*Arctotheca calendula*)
- Cotoneaster (*Cotoneaster pannosus*), (*C. lacteus*)
- Edible fig (*Ficus carica*)
- Fountain grass (*Pennisetum setaceum*)
- French broom (*Genista monspessulana*)
- Ice plant, sea fig (*Carpobrotus edulis*)
- Leafy spurge (*Euphorbia esula*)
- Myoporum (*Myoporum* spp.)
- Olive (*Olea europaea*)
- Pampas grass (*Cortaderia selloana*), and Andean pampas grass (*C. jubata*)
- Russian olive (*Elaeagnus angusticifolia*)
- Scotch broom (*Cytisus scoparius*) and striated broom (*C. striatus*)
- Spanish broom (*Spartium junceum*)
- Tamarix, salt cedar (*Tamarix chinensis*), (*T. gallica*), (*T. parviflora*), (*T. ramosissima*)
- Blue gum (*Eucalyptus globulus*)
- Athel tamarisk (*Tamarix aphylla*)

With the exception of poison oak, only those species listed in the Specific Plan's Suggested Plant List (Appendix D) shall not be planted anywhere on-site because they are invasive non-native plant species. Poison oak is a native plant species and could be used to deter human entrance to an area such as a mitigation/enhancement area.

CULTURAL RESOURCES

- CR-1** There is the potential that project construction will disturb previously unidentified buried archeological deposits and/or human remains. This is considered a Class II, significant but mitigable impact.

In addition to the provisions incorporated in the Specific Plan, the following mitigation measures would further reduce impacts related to cultural resources to less than significant levels.

- CR-1(a) Areas Not Surveyed.** All areas that were not surveyed by Conejo, as indicated in Figure 4.5-1, that will be subject to project-related earth disturbance shall be subject to archaeological survey prior to any such disturbances. This shall include APNs 076-481-014, 076-481-012, 076-491-003, 075-491-004, and 076-491-001, any planned trails or other developments within the areas designated as open space.
- CR-1(b) Righetti Hill.** Even though it is located within an area designated as open space, the top of Righetti Hill should be subject to archaeological survey. The City is responsible for the survey as part of any project to create a trail system that would provide access to the top of the hill by the general public.
- CR-1(c) Vegetation Clearance Monitoring.** Due to poor ground surface visibility, vegetation clearance/initial grading of the areas shown on Figure 4.5-2 should be monitored by an archaeologist. The archaeologist shall have the power to temporarily halt or redirect project construction in the event that potentially significant archaeological resources are exposed. Based on monitoring observations the lead archaeologist shall have the authority to refine the monitoring requirements as appropriate (i.e., change to spot checks, reduce the area to be monitored) in consultation with the lead agency. If potentially significant prehistoric or historic resources are exposed the lead archaeologist shall be responsible for evaluating the nature and significance of the find. If no archaeological resources are observed following the vegetation clearance/initial grading then no further monitoring shall be required. A monitoring report shall be provided to the City of San Luis Obispo and the CCIC.
- CR-1(d) Archaeological Resource Construction Monitoring.** At the commencement of project construction, an orientation meeting shall be conducted by an archaeologist for construction workers associated with earth disturbing procedures. The orientation meeting shall describe the possibility of exposing unexpected archaeological resources and directions as to what steps are to be taken if such a find is encountered.

An archaeologist shall monitor construction grading within 50 meters (164 feet) of the two isolated finds. In the event that prehistoric or historic archaeological resources are exposed during project construction, all earth disturbing work within 50 meters (164 feet) of the find must be temporarily suspended or redirected until an archaeologist has evaluated the nature and significance of the find. After the find has been appropriately mitigated (e.g.,

curation, preservation in place, etc.), work in the area may resume. The City should consider retaining a Chumash representative to monitor any field work associated with Native American cultural material.

If human remains are exposed, State Health and Safety Code Section 7050.5 requires that no further disturbance shall occur until the County Coroner has made the necessary findings as to origin and disposition pursuant to Public Resources Code Section 5097.98.

- CR-2** Project development will result in earth disturbance at several locations considered sensitive for archaeological resources. This is considered a Class II, significant but mitigable impact.

The following mitigation measure would reduce potential impacts related to identified archaeological resources to a less than significant level.

- CR-2(a) Subsurface Archaeological Testing.** If avoidance of an archaeological site(s) is not possible, a Subsurface Archaeological Resource Evaluation (SARE) shall be completed prior to issuance of a Land Use Permit. A SARE should be undertaken for Orcutt-1 with the following goals:

- a) Determine if there are intact subsurface deposits associated with this site;
- b) Determine the site's boundaries;
- c) Assess the site's integrity, i.e., is it intact or highly disturbed; and
- d) Evaluate the site's importance or significance.

The City should consider retaining a Chumash representative to monitor any subsurface testing/excavation at Orcutt-1. Results of the Phase 2 Evaluation will determine the need or lack thereof for additional data recovery and/or construction monitoring in the archaeological site area. When feasible, avoidance of impacts through project redesign is the preferred method for mitigating impacts to significant archaeological resources.

The archaeological excavation(s) shall be based on a written explicit research design that includes a statement or research objectives and a program for carrying out these objectives. All cultural materials collected shall be curated at a qualified institution that has proper facilities and staffing for insuring research access to the collections.

- CR-2(b) Construction Monitoring.** An archaeologist should monitor construction grading in the vicinity of the two isolated finds.

- CR-3** Implementation of the proposed project could result in indirect impacts to identified archaeological resources. This is considered a Class II, significant but mitigable impact

The following mitigation measure would reduce potential indirect impacts related to identified archaeological resources to a less than significant level.

- CR-3(a) Prohibition of Archaeological Site Tampering.** Off-road vehicle use, unauthorized collecting of artifacts, and other activities that could destroy or

damage archaeological or cultural sites shall be prohibited. Signs shall be posted on the property to discourage these types of activities and warn of trespassing violations and imposed fines.

CR-4 Implementation of the proposed project could result in indirect impacts to historical resources. This is considered a Class II, significant but mitigable impact.

CR-4(a) Historical Evaluation. Prior to development, a qualified historian should be retained to conduct a historical evaluation of the 50+ year old structures within the Orcutt Area using the City's Historic Preservation Program Guidelines. Any structure determined to be an important/ significant historic resource shall be mitigated as appropriate prior to its demolition or relocation. The historic structure evaluation should include the history of the Skinner/Righetti Ranch and the ranch complex should be recorded on appropriate DPR forms. Finally, the historian shall determine if project development will have any significant direct or indirect impacts on the Bettencourt/Rodriguez Adobe, a city historic landmark located immediately adjacent to the Orcutt Area.

NOISE

N-1 Construction under the Specific Plan would temporarily generate high noise levels on-site. Because noise could exceed thresholds in the City General Plan Noise Element, impacts are considered Class II, significant but mitigable.

Implementation of the policy and programs included in the Specific Plan would reduce impacts to noise generated from temporary construction. In addition to the policies and programs, the following mitigation measure is required to reduce construction noise impacts on nearby residences:

N-1(a) Compliance with City Noise Ordinance. Construction hours and noise levels shall be compliant with the City Noise Ordinance [Municipal Code Chapter 9.12, Section 9.12.050(6)]. Methods to reduce construction noise can include, but are not limited to, the following:

- Equipment Shielding. Stationary construction equipment that generates noise can be shielded with a barrier.
- Diesel Equipment. All diesel equipment can be operated with closed engine doors and equipped with factory-recommended mufflers.
- Electrical Power. Whenever feasible, electrical power can be used to run air compressors and similar power tools.
- Sound Blankets. The use of sound blankets on noise generating equipment.

N-4 The proposed Specific Plan would place additional sensitive receptors in the vicinity of the Union Pacific Railroad tracks, exposing them to noise levels that could potentially exceed City noise standards. This is considered a Class II, significant but mitigable, impact.

The Orcutt Area Specific Plan includes goals, policies, and programs that are intended to reduce noise impacts caused by the nearby railroad.

In addition to the provisions proposed in the Specific Plan, the following mitigation measures are required to reduce UPRR noise impacts on nearby residences:

N-4(a) Specific Plan Revision. The Specific Plan shall be revised to meet the noise standards of the City General Plan Noise Element. Policy 4.5.1a shall be revised to require that outdoor noise levels for residences not exceed 60 dB (Ldn) and indoor noise levels for residences and schools not exceed 45 dB (Ldn). Program 4.5.2a shall also be revised to ensure that these standards are met. Indoor noise levels can be reduced using the design and materials techniques described in Specific Plan Programs 4.5.1a, 4.5.1b, 4.5.1c, 4.5.1d, 4.5.1e, 4.5.1f, 4.5.2a, 4.5.2b, and 4.5.2c. Outdoor noise levels can be reduced in the following ways:

- a) Locate all proposed residential and school development outside of the 60 Ldn contour line (352 feet from the centerline of the railroad); or
- b) For any residential or school development located within 352 feet of the railroad centerline, a combination of barrier methods specified in the Noise Element must be implemented. Residential or school project applicants in this area shall demonstrate to the satisfaction of the Community Development Department that proposed development will not be exposed to outdoor noise levels that exceed Noise Element standards. Because of the varying topography of the site relative to the railroad tracks, and the fact the development design has not been determined, the specific attenuation methods cannot be definitively determined. Options could include one or more of the following approaches:
 - Berm or wall along the railroad right-of-way, which would likely vary in height from about 8 to 20 feet, based on preliminary noise models included in this EIR;
 - Design of individual homes such that structures block the line-of-sight from useable backyards to the railroad tracks;
 - For homes with backyards not blocked by intervening structures, backyard fencing of sufficient height to block line-of sight to railroad tracks.

The design of noise barriers and backyard layouts and walls shall be examined by an approved noise consultant, to determine if they provide sufficient mitigation to comply with Noise Element standards related to outdoor noise exposure.

APPENDIX D – SUGGESTED PLANT LIST

The following plant lists are not inclusive, they are recommendations. The lists are provided to assist the landscape designer and/or developer for plant selection.

Low water use trees: (Western U.S. native species are noted as WN)

- | | |
|---|---|
| <input type="checkbox"/> Acacia species | <input type="checkbox"/> Grevillea robusta (Silk oak) |
| <input type="checkbox"/> Aesculus californica (California buckeye) WN | <input type="checkbox"/> Koelreuteria paniculata (Goldenrain tree) |
| <input type="checkbox"/> Albizia julibrissin (Silk tree) | <input type="checkbox"/> Lyonothamnus floribundus asplenifolius (Fernleaf Catalina ironwood) WN |
| <input type="checkbox"/> Arbutus unedo (Strawberry madrone) | <input type="checkbox"/> Maclura pumifera (Osage orange) |
| <input type="checkbox"/> Arbutus menziesii (Madrone) WN | <input type="checkbox"/> Parkinsonia aculeata (Jerusalem thorn) |
| <input type="checkbox"/> Brahea armata (Mexican blue palm) WN | <input type="checkbox"/> Pinus jeffreyi (Jeffrey pine) WN |
| <input type="checkbox"/> Callistemon citrinus (Lemon bottlebrush) | <input type="checkbox"/> Pinus ponderosa (Ponderosa Pine) WN |
| <input type="checkbox"/> Casuarina equisetifolia (Horsetail beefwood) | <input type="checkbox"/> Pinus sabiniana (Digger pine) WN |
| <input type="checkbox"/> Celtis (Hackberry) | <input type="checkbox"/> Pistacia species |
| <input type="checkbox"/> Ceratonia siliqua (Carob, St. John's bread) | <input type="checkbox"/> Populus fremontii (Fremont cottonwood) |
| <input type="checkbox"/> Cercidium (Palo verde) WN | <input type="checkbox"/> Prunus ilicifolia (Hollyleaf Cherry) WN |
| <input type="checkbox"/> Cercis occidentalis (Western redbud) WN | <input type="checkbox"/> Quercus agrifolia (Coast live oak) WN |
| <input type="checkbox"/> Eriobotrya japonica (Loquat) | <input type="checkbox"/> Quercus chrysolepis (Canyon live oak) WN |
| <input type="checkbox"/> Fraxinus oxycarpa (Raywood ash) WN | |
| <input type="checkbox"/> Geijera parviflora (Australian willow) | |
| <input type="checkbox"/> Melia azedarach (China-berry) | |
| <input type="checkbox"/> Melaleuca species | |

Low water use shrubs: (Western U.S. native species are noted as WN)

- | | |
|---|--|
| <input type="checkbox"/> Acacia (many) | <input type="checkbox"/> Lavandula species |
| <input type="checkbox"/> Arbutus unedo (Strawberry madrone) | <input type="checkbox"/> Lavatera assurgentiflora |
| <input type="checkbox"/> Arbutus menziesii (Madrone) WN | <input type="checkbox"/> Leucophyllum frutescens |
| <input type="checkbox"/> Arctostaphylos species WN | <input type="checkbox"/> Lysiloma thornberi |
| <input type="checkbox"/> Artemisia species WN | <input type="checkbox"/> Mahonia aquifolium (Oregon grape) WN |
| <input type="checkbox"/> Baccharis pilularis (Dwarf coyote brush) WN | <input type="checkbox"/> Mahonia pinnata (California Holly grape) WN |
| <input type="checkbox"/> Carpenteria californica (Bush anemone) WN | <input type="checkbox"/> Mahonia repens (Creeping Mahonia) WN |
| <input type="checkbox"/> Caesalpinia gilliesii (Bird of paradise bush) ?? | <input type="checkbox"/> Melaleuca species |
| <input type="checkbox"/> Caragana arborescens (Siberian peashrub) | <input type="checkbox"/> Nerium oleander |
| <input type="checkbox"/> Cassia artemisioides (Feathery cassia) | <input type="checkbox"/> Photinia serrulata |
| <input type="checkbox"/> Catha edulis (Khat) | <input type="checkbox"/> Pinus edulis |
| <input type="checkbox"/> Ceanothus species WN | <input type="checkbox"/> Pinus monophylla |
| <input type="checkbox"/> Cercis occidentalis (Western redbud) WN | <input type="checkbox"/> Pittosporum |
| <input type="checkbox"/> Cercocarpus (Mountain mahogany) WN | <input type="checkbox"/> Plumbago auriculata |
| <input type="checkbox"/> Chamaerops humilis (Mediterranean fan palm) | <input type="checkbox"/> Portulacaria afra |
| <input type="checkbox"/> Chamelaucium uncinatum (Geraldton waxflower) | <input type="checkbox"/> Prosopis glandulosa torreyana |
| <input type="checkbox"/> Cistus species | <input type="checkbox"/> Prunus caroliniana |
| <input type="checkbox"/> Convolvulus cneorum (Bush morning glory) | <input type="checkbox"/> Prunus ilicifolia |
| <input type="checkbox"/> Coprosma kirkii (Kirk's coprosma) | <input type="checkbox"/> Prunus lyoni |
| <input type="checkbox"/> Cotinus coggygria | <input type="checkbox"/> Punica granud |
| <input type="checkbox"/> Crassula argentea | <input type="checkbox"/> Pyracantha |
| <input type="checkbox"/> Crassula falcata | <input type="checkbox"/> Rhamnus alaternus |
| <input type="checkbox"/> Cupressus glabra | <input type="checkbox"/> Rhamnus californica |
| <input type="checkbox"/> Dalea spinosa | <input type="checkbox"/> Rhamnus crocea ilicifolia |
| <input type="checkbox"/> Dendromecon | <input type="checkbox"/> Rhus ovata |
| <input type="checkbox"/> Dodonaea viscosa | <input type="checkbox"/> Rosa rugosa |
| <input type="checkbox"/> Echium | <input type="checkbox"/> Rosmarinus |
| <input type="checkbox"/> Escallonia | <input type="checkbox"/> Salvia clevelandii |
| <input type="checkbox"/> Fallugia paradoxa | <input type="checkbox"/> Salvia leucantha |
| <input type="checkbox"/> Fremontodendron | <input type="checkbox"/> Santolina chamaecyparissus |
| <input type="checkbox"/> Garrya | <input type="checkbox"/> Simmondsia chinensis |
| <input type="checkbox"/> Grevillea | <input type="checkbox"/> Sollya heterophylla |
| <input type="checkbox"/> Hakea | <input type="checkbox"/> Taxus |
| <input type="checkbox"/> Heteromeles arbutifolia | <input type="checkbox"/> Teucrium |
| <input type="checkbox"/> Hypericum calycinum | <input type="checkbox"/> Trichostema lanatum |
| <input type="checkbox"/> Lagerstroemia indica | <input type="checkbox"/> Xylosma congestum |
| <input type="checkbox"/> Lantana species | |

ORCUTT AREA SPECIFIC PLAN

Appendix D

Low water use perennials, bulbs, annuals: (Alternatives to turf.)

- | | |
|--|--|
| <input type="checkbox"/> Achillea | <input type="checkbox"/> Leucocoryne ixioides |
| <input type="checkbox"/> Agave | <input type="checkbox"/> Liatris |
| <input type="checkbox"/> Aloe arborescens | <input type="checkbox"/> Limonium perezii |
| <input type="checkbox"/> Amaryllis belladonna | <input type="checkbox"/> Linum |
| <input type="checkbox"/> Anacyclus depressus | <input type="checkbox"/> Marrubium vulgare |
| <input type="checkbox"/> Baccharis pilularis (Dwarf coyote brush) WN | <input type="checkbox"/> Mimulus |
| <input type="checkbox"/> Baptisia australis | <input type="checkbox"/> Narcissus |
| <input type="checkbox"/> Centranthus ruber | <input type="checkbox"/> Oenothera berlandieri |
| <input type="checkbox"/> Cleome spinosa | <input type="checkbox"/> Phlomis fruticosa |
| <input type="checkbox"/> Coreopsis | <input type="checkbox"/> Phormium |
| <input type="checkbox"/> Dietes vegeta | <input type="checkbox"/> Polygonum cuspidatum compactum |
| <input type="checkbox"/> Dudleya brittonii | <input type="checkbox"/> Portulaca grandiflora |
| <input type="checkbox"/> Echeveria (most) | <input type="checkbox"/> Puya berteroniana |
| <input type="checkbox"/> Erogonum | <input type="checkbox"/> Romneya coulteri |
| <input type="checkbox"/> Euryops | <input type="checkbox"/> Sedum (many) |
| <input type="checkbox"/> Gaillardia | <input type="checkbox"/> Sisyrinchium bellum |
| <input type="checkbox"/> Hippocrepis comosa | <input type="checkbox"/> Tithonia rotundifolia |
| <input type="checkbox"/> Iris, bearded | <input type="checkbox"/> Verbena |
| <input type="checkbox"/> Iris, Pacific Coast natives | <input type="checkbox"/> Yucca (most) |
| <input type="checkbox"/> Kniphofia uvaria | <input type="checkbox"/> Zauschneria californica (California Fuchsia) WN |
| <input type="checkbox"/> Leonotis leonurus | |

The Orcutt Specific Plan Area also includes riparian and creek corridors. These corridors support native and non-native plant material adapted to wet growing conditions. Following is a list of plants that are acceptable for planting in or near the riparian corridor.

Riparian trees:

- | | |
|--|---|
| <input type="checkbox"/> Alnus rhombifolia (White Alder) | <input type="checkbox"/> P. racemosa (California Sycamore) |
| <input type="checkbox"/> Acer macrophyllum (Bigleaf Maple) | <input type="checkbox"/> Populus fremontii (Western Cottonwood) |
| <input type="checkbox"/> Acer negundo (Box Elder) | <input type="checkbox"/> P. trichocarpa (Black Cottonwood) |
| <input type="checkbox"/> Heteromeles arbutifolia (Toyon, California Holly) | <input type="checkbox"/> Quercus agrifolia (Coast Live Oak) |
| <input type="checkbox"/> Juglans hindsii (California Black Walnut) | <input type="checkbox"/> Robinia pseudoacacia [& hybrids] (Black Locust) |
| <input type="checkbox"/> Pinus radiata. (Monterey Pine) | <input type="checkbox"/> Salix spp. (willows) |
| <input type="checkbox"/> P. sabiniana (Gray Pine) | <input type="checkbox"/> Sequoia sempervirens (Coast Redwood) |
| <input type="checkbox"/> P. occidentalis (American Sycamore) | <input type="checkbox"/> Umbellularia californica (California Bay-laurel) |

Riparian shrubs:

- | | |
|---|--|
| <input type="checkbox"/> Baccharis pilularis (Coyote Brush) | <input type="checkbox"/> Rosa californica (Wild Rose) |
| <input type="checkbox"/> Cornus californica (Creek Dogwood) | <input type="checkbox"/> Rubus ursinus (California Blackberry) |
| <input type="checkbox"/> C. stolonifera (Redtwig Dogwood) | <input type="checkbox"/> R. vitifolius (California Blackberry) |
| <input type="checkbox"/> Heteromeles arbutifolia (Toyon, California Holly) | <input type="checkbox"/> Salix spp. (willows) |
| <input type="checkbox"/> Myrica californica (Bayberry, California Wax Myrtle) | <input type="checkbox"/> Sambucus caerulea mexicana (Elderberry) |
| <input type="checkbox"/> Rhamnus californica (Coffeeberry) | <input type="checkbox"/> Solanum douglasii (Black Nightshade) |
| <input type="checkbox"/> Ribes speciosum (Fuchsia-flowered Gooseberry) | |

Riparian herbs:

- | | |
|--|--|
| <input type="checkbox"/> Apocynum cannabinum (Dogbane) | <input type="checkbox"/> Juncus, spp. (toles) |
| <input type="checkbox"/> Carex spp. (sedges) | <input type="checkbox"/> Mimulus guttatus (Monkeyflower) |
| <input type="checkbox"/> Distichlis spp. (Saltgrass) | <input type="checkbox"/> Polygonum coccineum |
| <input type="checkbox"/> Epilobium spp. (willow herbs) | <input type="checkbox"/> Typha spp. (cattails) |
| <input type="checkbox"/> Equisetum spp. (Common horsetail) | <input type="checkbox"/> Urtica spp. (nettles) |

Afternoon wind plays an important role in the microclimate of the Orcutt Specific Plan Area. Prevailing winds flow from the north coast areas through the Plan Area towards the southeast. The following plant species are recommended to moderate the northwest winds.

Wind moderating trees:

- | | |
|--|--|
| <input type="checkbox"/> Acacia melanoxylon | <input type="checkbox"/> Picea abies |
| <input type="checkbox"/> Calocedrus decurrens | <input type="checkbox"/> Pinus species |
| <input type="checkbox"/> Casuarina stricta | <input type="checkbox"/> Pittosporum (all but P. phillyraeoides) |
| <input type="checkbox"/> Celtis (Hackberry) | <input type="checkbox"/> Populus nigra 'Italica' |
| <input type="checkbox"/> Chamaecyparis lawsoniana (Ellwood cypress) WN | <input type="checkbox"/> Prosopis glandulosa torreyana |
| <input type="checkbox"/> Cupressocyparis leylandii | <input type="checkbox"/> Pseudotsuga menziesii |
| <input type="checkbox"/> Cupressus glabra | <input type="checkbox"/> Sequoia sempervirens |
| <input type="checkbox"/> Cupressus macrocarpa | <input type="checkbox"/> Tamarix aphylla |
| <input type="checkbox"/> Lagunaria patersonii | <input type="checkbox"/> Thuja plicata |
| <input type="checkbox"/> Ligustrum lucidum | <input type="checkbox"/> Ulmus pumila |

Wind moderating shrubs:

- | | |
|--|---|
| <input type="checkbox"/> Ligustrum japonicum 'Texanum' | <input type="checkbox"/> Pyracantha (tall growing ones) |
| <input type="checkbox"/> Melaleuca | <input type="checkbox"/> Rhamnus alaternus |
| <input type="checkbox"/> Nerium oleander | <input type="checkbox"/> Taxus baccata 'Stricta' |
| <input type="checkbox"/> Pittosporum (all but P. napaulense) | <input type="checkbox"/> Taxus cuspidata |
| <input type="checkbox"/> Prunus caroliniana | <input type="checkbox"/> Taxus media |
| <input type="checkbox"/> Prunus laurocerasus | <input type="checkbox"/> Tecomaria capensis |
| <input type="checkbox"/> Prunus lyonii | <input type="checkbox"/> Thuja occidentali |

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